

MANCHESTER-BOSTON REGIONAL AIRPORT PARKING GARAGE: LEVEL-6 FLOOR & LEVEL-5 CEILING SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS (FY24-805-21)



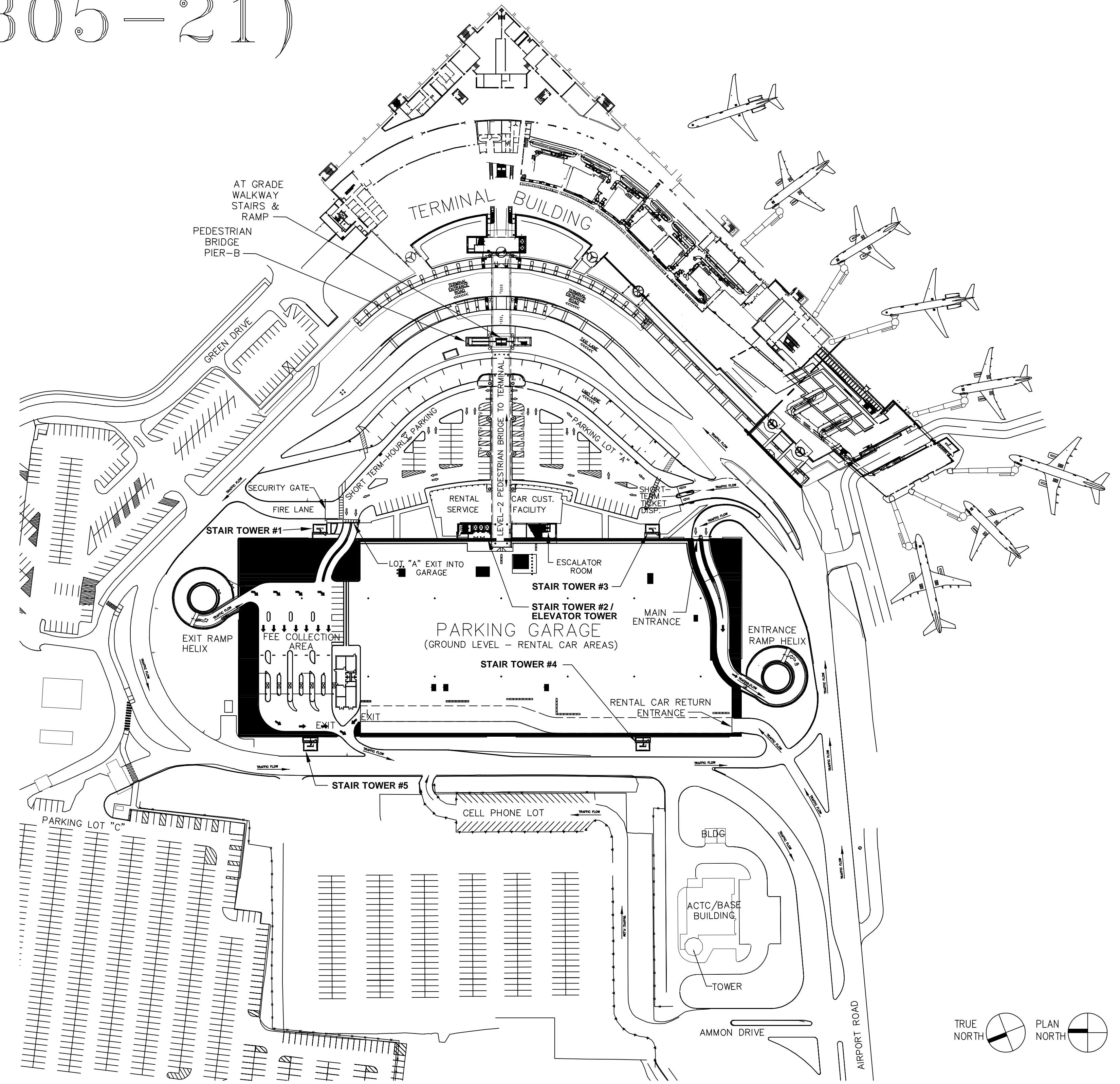
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1155 ELM STREET, SUITE 401
MANCHESTER, NH 03101
PROJECT # 60719585-T03

LIST OF DRAWINGS

SHEET #	DRAWING NO.	TITLE
GENERAL:		
1	TS.24-1	TITLE SHEET & AIRPORT SITE PLAN
2	GN.24-1	GENERAL NOTES & WORK ITEM LIST
3	PZ.24-5/6	PHASING AND TRAFFIC PLAN: LEVEL 5 & 6
REPAIR PLANS:		
4	RP.24-FLR.6N	REPAIR PLAN: LEVEL 6 - FLOOR / NORTH END
5	RP.24-FLR.6S	REPAIR PLAN: LEVEL 6 - FLOOR / SOUTH END
6	RP.24-CLG.5N	DRIVE BAY & CEILING REPAIR PLAN: LEVEL-5 CEILING (NORTH)
7	RP.24-CLG.5S	DRIVE BAY & CEILING REPAIR PLAN: LEVEL-5 CEILING (SOUTH)
REPAIR DETAILS:		
8	RD.24-F1	FLOOR REPAIR DETAILS - I
9	RD.24-F2	FLOOR REPAIR DETAILS - II
10	RD.24-F3	FLOOR REPAIR DETAILS - III
11	RD.24-F4	FLOOR REPAIR DETAILS - IV DRIVE BAY JOINTLINE REBUILD
12	RD.24-C1	CEILING REPAIR DETAILS - I
13	RD.24-C2	CEILING REPAIR DETAILS - II



AIRPORT SITE PLAN - GARAGE AREA

SCALE: 1:100



ABBREVIATIONS:

Table of abbreviations including ACI (American Concrete Institute), ADD'L (Additional), ADJ. (Adjacent), ALT. (Alternate), ASTM (American Society for Testing and Materials), AWS & @ (American Welding Society and At), BIT. (Bituminous), BM. (Beam), B.O. (Bottom of), BOT. (Bottom), BRG. (Bearing), C.F. (Cubic Feet), C.I.P. (Cast-in-place), C.J. (Construction Joint), CMU (Concrete Masonry Unit), COL.(S) (Column(s)), CONC. (Concrete), CONN. (Connection), CONST. (Construction), CONT. (Continuous), CTR. (Center), DIA. (Diameter), DEMO. (Demolish), DET. (Detail), DIAG. (Diagonal), D.T. (Double Tee), DWG.(S) (Drawing(s)), DWL.(S) (Dowel(s)), D/D (Dust/Debris), EA. (Each), E.F. (Each Face), E.J. (E/J) (Expansion Joint), EL. (Elevation), ELEC. (Electrical), EMBED. (Embedment), EQ. (Equal), E.W. (Each Way), EWEF (Each Way, Each Face), EXIST. (Existing), EXP. (Expansion), EXT. (Exterior), FIN. (Finished), FL. (Floor), FND. (Foundation), FT. (Foot (feet)), FTG. (Footing), GA. (Gage), GAL. (Gallon(s)), GALV. (Galvanized), G.C. (General Contractor), GR. (Grade), H.A.S. (Headed Anchor Stud), HDG (Hot Dip Galvanized), HDP (High-Density Polyethylene), H.E.F. (Horizontal Each Face), HORIZ. (Horizontal), H.P. (High Point), HSS (Hollow Structural Sections), IN. (Inch(es)), INFO. (Information), INV. (Inverted), I.T. (Inverted Tee)

SYMBOLS

Table of symbols including = (Equals), > (Greater than), < (Less than), >= (Greater than or equal), <= (Less than or equal), % (Percent), +/- (Plus/Minus), # (Number), f'c (Specified 28-day design compressive strength of concrete), Fy (Specified yield strength), ø (Diameter), #4@18 (Reinforcing bar size and spacing)

LEGEND:

1.1 BUBBLE NUMBER REFERS TO SPECIFIC REPAIR DETAIL IN THE DETAIL SHEETS AND/OR REFERS TO SPECIFICATIONS SECTION 02000 "WORK ITEMS".

GENERAL NOTES - PARKING GARAGE REPAIRS:

- *CONSTRUCTION DOCUMENTS INCLUDE THE PLANS AND DETAILS ON THE FOLLOWING DRAWINGS AND SEPARATELY BOUND SPECIFICATIONS.
1. CONSTRUCTION:
A. CONSTRUCTION SHALL BE IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE OF NEW HAMPSHIRE AND CITY OF MANCHESTER CODES AND ORDINANCES, INCLUDING FIRE CODES.
B. ALL DRAIN AND PIPING WORK SHALL CONFORM TO LOCAL BUILDING CODES.
C. ALL MATERIAL PROPERTIES SHALL BE AS NOTED IN THE SPECIFICATION.
D. COORDINATE ALL UTILITIES SHUTDOWNS WITH OWNER PRIOR TO INTERRUPTING SERVICES.
E. CONTRACTOR SHALL CONFORM TO TRAFFIC FLOW COORDINATION AND AREA CLOSURE REQUIREMENTS CONTAINED IN THE CONTRACT DOCUMENTS.
F. CONTRACTOR SHALL LIMIT FLOOR LOADING WITHIN THE WORK AREA TO NO MORE THAN 50 PSF. VEHICLE WHEEL LOADS SHALL BE LIMITED TO A MAXIMUM OF 2,000 LBS OVER A 20 SQ. IN. TIRE CONTACT AREA (FLOOR DESIGN LOAD). CONTRACTOR SHALL NOT STORE SUPPLIES OR PARK VEHICLES OVER ANY SHORED AREAS.
G. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD.
2. INTENT OF THE REPAIR DRAWINGS:
A. THE INTENT OF THE STRUCTURAL REPAIR DRAWINGS IS TO SHOW THE EXISTING STRUCTURAL FEATURES AND THE GENERAL LOCATIONS OF REPAIR WORK.
B. BUBBLE NUMBER REFERS TO SPECIFIC REPAIR DETAIL IN THE DETAIL SHEETS AND/OR WORK ITEM IDENTIFIED IN SECTION 02000 OF THE PROJECT SPECIFICATIONS "WORK ITEMS". NOT ALL ITEMS ARE REPRESENTED WITH A DRAWING DETAIL.
C. WHERE WORK ITEM BUBBLE IS NOTED TYP., IT MEANS THE WORK IS TYPICAL THROUGHOUT THE REFERENCED WORK AREA.
D. WHERE WORK ITEM BUBBLE IS NOTED WITH T.A.R., THE WORK IS TYPICAL AS REQUIRED AND OCCURS AT AREAS IN ADDITION TO THE DESIGNATED LOCATION.
E. CONTRACTOR SHALL ESTABLISH EXTENT AND LOCATION OF WORK AND REPAIR AREA AND BE VERIFIED BY THE ENGINEER PRIOR TO COMMENCEMENT OF WORK AT EACH AREA.
F. REFER TO THE UNIT PRICE BID FORM FOR APPROXIMATED QUANTITIES FOR EACH WORK ITEM, AS APPLICABLE. QUANTITIES MAY VARY AND MAY BE INCREASED, DECREASED OR DELETED AT THE DISCRETION OF THE ENGINEER.
3. CONCRETE:
A. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI (28 DAY) CONFORMING TO NHDOT CLASS AA, AND SHALL BE HIGH EARLY STRENGTH MIX FOR A MINIMUM OF 3,000 PSI IN 5 DAYS. BAGGED MATERIAL MAY BE USED SUBJECT TO PRODUCT DATA, SUBMITTAL TO, AND APPROVAL BY, THE ENGINEER.
B. EXPOSED CONCRETE EDGES SHALL HAVE A 1/2" CHAMFER.
C. CONCRETE EXPOSED TO VIEW SHALL HAVE SMOOTH FORM FINISH WITH FINS AND FORM MARKS REMOVED TO A SMOOTH, STONE RUBBED SURFACE.
D. ALL REINF. SHALL MEET ASTM A615 REQUIREMENTS.
E. REINFORCEMENT INTENDED TO BE WELDED SHALL MEET ASTM A706 (REQUIREMENTS FOR WELDABLE REINFORCEMENT).
F. AT EXPOSED REINFORCEMENT WHERE CROSS-SECTIONAL LOSSES DUE TO DETERIORATION EXCEED 20 PERCENT, SPLICE WITH REINF. MATCHING EXISTING REINF. BAR SIZE. EXTEND LAP SPLICE REINFORCEMENT BEYOND DETERIORATED SECTION TO DEVELOP PROPER TENSION SPLICE OR, AT ENGINEER'S DISCRETION, CONTRACTOR MAY BE DIRECTED TO USE MECHANICAL COUPLERS IN LIEU OF LAP SPLICE.
G. ALL EXPOSED REINF. SHALL BE CLEANED BY SAND BLASTING, WIRE SCABBLING TO REMOVE ALL TRACES OF RUST DOWN TO WHITE METAL.
H. COAT ALL EXPOSED REINF. WITH APPROVED EPOXY COATING.
4. REMOVAL OF MATERIAL:
A. ALL MATERIAL REMOVED SHALL BE DONE WITH EXTREME CAUTION.
B. FOR REMOVAL OF MATERIAL AT AREAS TO BE PATCHED, PROVIDE NEAT SAWCUT TO 1" MAXIMUM DEPTH AT EDGES TO REMAIN.
C. NOTIFY ENGINEER IMMEDIATELY OF ANY FIELD CONDITIONS WHICH APPEAR TO DIFFER FROM THOSE SHOWN OR IMPLIED ON THE DRAWINGS.
D. RESTORE PRECAST PIECES REQUIRING STRUCTURAL REPAIR.
E. PROPERLY DISPOSE OF ALL MATERIALS REMOVED FROM THE EXISTING STRUCTURE.
5. PAY ITEMS:
A. QUANTITIES FOR PAY ITEMS MUST BE VERIFIED BY THE FIELD ENGINEER AND/OR AUTHORIZED REPRESENTATIVE AND CONFIRMED BY THE GENERAL CONTRACTOR PRIOR TO SUBMISSION OF PAYMENT APPLICATIONS.

PHASING AND WORK ZONE CAPTURE NOTES:

- 1. THE PROJECT SHALL BE PERFORMED IN (2) PHASES AS SHOWN ON THE PLANS AND DESCRIBED AS FOLLOWS:
PHASE 6S-F / 5S-C (LEVEL 6 SOUTH FLOOR & LEVEL 5 SOUTH CEILING): WORK AREA GENERALLY EXTENDS FROM COLUMN LINE 14.0 SOUTHWARD TO COLUMN LINE 26.
PHASE 6N-F / 5N-C (LEVEL 6 NORTH FLOOR & LEVEL 5 NORTH CEILING): WORK AREA GENERALLY EXTENDS FROM COLUMN LINE 14.0 NORTHWARD TO COLUMN LINE 2.
2. THE OWNER (AIRPORT) WILL CLOSE AND CAPTURE THE WORK AREAS ON LEVEL-6 AND LEVEL-5 IN ADVANCE OF THE CONTRACTOR'S WORK IN ACCORDANCE WITH THE PROJECT MILESTONE SCHEDULE.
3. THE OWNER WILL PLACE BARRICADES AND SIGNAGE AT THE HELIX ENTRANCES AND PEDESTRIAN ENTRANCES TO THE WORK AREAS. THE CONTRACTOR SHALL GIVE THE OWNER AT LEAST 2 WEEKS ADVANCE WRITTEN NOTICE (WITH EXPLANATION) IF THE MILESTONE SCHEDULE START-DATE OF A PHASE CANNOT BE MET.
4. THE OWNER WILL FURNISH AND INSTALL VEHICULAR ACCESS LANE AND PEDESTRIAN WALKWAY TRAFFIC CONTROL DEVICES (BARRELS, ROPING, ETC.) AS ILLUSTRATED ON THE PLANS TO FACILITATE THE PERFORMANCE OF THE WORK. THE CONTRACTOR SHALL COORDINATE HIS WORK WITH THE OWNER TO VERIFY AND MONITOR THE ACCURACY OF SAFETY AREA PLACEMENT AROUND AND UNDER HIS WORK TO ENSURE THE SAFETY OF THE GENERAL PUBLIC AND PROTECTION OF PARKED PATRON VEHICLES. CONTRACTOR SHALL MAINTAIN AND/OR ADJUST DEVICES TO SUIT THE WORK AREAS DELINEATION AND PUBLIC SAFETY.
5. THE OWNER WILL FURNISH AND INSTALL DIRECTIONAL SIGNAGE AS INDICATED ON THE PLANS AND AS REQUIRED FOR PUBLIC USE OF THE FACILITY DURING CONSTRUCTION. THE CONTRACTOR SHALL FURNISH AND INSTALL CONSTRUCTION HAZARD WARNING SIGNAGE SUBSIDIARY TO ITEM 1.1 GENERAL REQUIREMENTS (SEE TECHNICAL SPECIFICATION SECTION 02000).
6. PUBLIC PEDESTRIAN ACCESS TO THE ELEVATOR LOBBY SHALL BE MAINTAINED AT ALL TIMES. SUB-PHASING OF LOCALIZED WORK AREAS IN FRONT OF THE DOORS WILL BE REQUIRED TO BE PERFORMED BY THE CONTRACTOR.
7. THE CONTRACTOR SHALL NOT BEGIN THE WORK IN THE NORTH PHASE AREA UNTIL SPECIFICALLY AUTHORIZED TO DO SO BY THE OWNER. THE WORK IN THE SOUTH PHASE AREA MUST BE COMPLETE AND PREPARED FOR PUBLIC PARKING PRIOR TO RELOCATING THE TRAFFIC CONTROL AND BEGINNING GENERAL WORK IN THE NEXT PHASE AREA.
8. ISOLATED (UNOCCUPIED) SUB-WORK AREAS (AND SAFETY AREAS) IN THE NEXT PHASE AREA MAY BE CAPTURED BY THE CONTRACTOR, SUBJECT TO THE APPROVAL OF THE OWNER, AND LOCALIZED PREPARATORY WORK INITIATED DURING THE PARKED VEHICLE ATTRITION PERIOD (APPROX. 2 WEEKS) JUST PRIOR TO THE GENERAL CHANGE FROM THE SOUTH PHASE TO THE NORTH PHASE.

WORK ITEM LIST FOR WORK ITEM DEFINITIONS, SEE PROJECT SPECIFICATIONS SECTION 02000

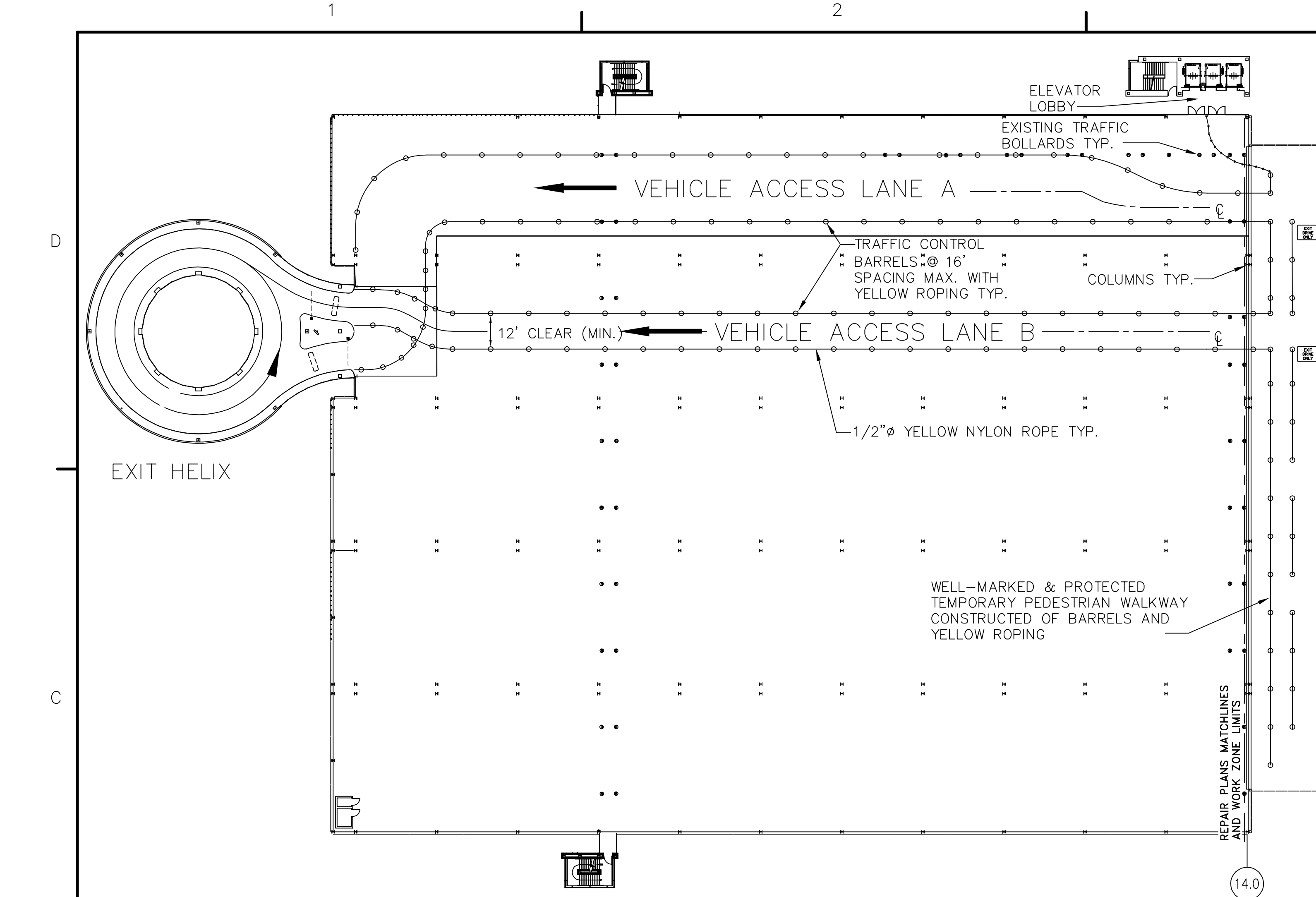
Table with columns: WORK ITEM, DESCRIPTION, UNITS. Rows include: 1.0 GENERAL REQUIREMENTS (1.1 GENERAL REQUIREMENTS - MOBILIZATION AND SUPPLEMENTARY GENERAL CONDITIONS* LS); 2.0 FLOOR REPAIRS (2.1 FLOOR REPAIR - FULL DEPTH AT C.I.P CONCRETE WASH - DRIVE BAYS SF; 2.2 FLOOR REPAIR - PARTIAL DEPTH AT C.I.P CONCRETE WASH SF; 2.3 FLOOR REPAIR - PARTIAL DEPTH - AT PRECAST CONCRETE FLOOR SLAB SF; 2.6 FLOOR REPAIR - TRAFFIC TOPPING - PEDESTRIAN GRADE* SF; 2.7A FLOOR REPAIR - TRAFFIC TOPPING - FULL MEMBRANE SYSTEM* SF; 2.7A-R FLOOR REPAIR - TRAFFIC TOPPING - FULL MEMBRANE SYSTEM REPAIR AREA* SF; 2.7B FLOOR REPAIR - TRAFFIC TOPPING - WEARCOAT AND UV-TOPCOAT SF; 2.7J FLOOR REPAIR - NYLON REINFORCED TRAFFIC TOPPING (1FT WIDE) AT JOINTLINE PRECAST TEE-TO-TEE FLANGE SF; 2.7H-R FLOOR REPAIR - TRAFFIC TOPPING MEMBRANE REPAIRS AT ENTRANCE & EXIT HELIX SF; 2.7H-T FLOOR REPAIR - TRAFFIC TOPPING - WEARCOAT & UV - TOPCOAT - HELIX SF; 2.7UV FLOOR REPAIR - TRAFFIC TOPPING - UV-TOPCOAT* SF; 2.8 FLOOR REPAIR - PENETRATING CONCRETE SEALER WITH MIGRATING CORROSION INHIBITOR (MCI)* SF); 3.0 CEILING REPAIRS (3.3 CEILING REPAIR - EPOXY CRACK INJECTION LF; 3.4 CEILING REPAIR - OVERHEAD CONCRETE MORTAR REPAIR SF; 3.5 CEILING REPAIR - GFRP FABRIC WRAP* SF; 3.6 CEILING REPAIR - PENETRATING CORROSION INHIBITOR* SF; 3.8 CEILING REPAIR - GALVANIC CORROSION PROTECTION EA; 3.9 CEILING REPAIR - BLAST CLEANING W/ ABRASIVE MEDIA* SF); 5.0 FLOOR SLAB JOINT REPAIRS (5.2A FLOOR SLAB JOINT REPAIR - EXPANSION JOINT NOSING - SURFACE REPAIR LF; 5.3 FLOOR SLAB JOINT REPAIR - REMOVE AND REPLACE FAILED T/T FLANGE JOINT SEALANTS LF; 5.5 FLOOR SLAB JOINT REPAIR-PRECAST TEE ENDJOINT REPLACEMENT-C.I.P. WASH LF; 5.6 FLOOR SLAB JOINT REPAIR - REMOVE AND INSTALL COVE SEALANT LF); 8.0 BOLLARDS & TRAFFIC CONTROL (8.1 BOLLARDS & TRAFFIC CONTROL - CONCRETE BOLLARD REMOVAL EA; 8.2 BOLLARDS & TRAFFIC CONTROL - CONCRETE BOLLARD DISPOSAL* LS; 8.3 BOLLARDS & TRAFFIC CONTROL - PROVIDE TRAFFIC DELINEATOR DEVICES (MATERIAL)* EA; 8.4 BOLLARDS & TRAFFIC CONTROL - INSTALL TRAFFIC DELINEATOR DEVICES EA; 8.5 BOLLARDS & TRAFFIC CONTROL - REINSTALL SALVAGED CONCRETE BOLLARD EA); 9.0 FIELD ITEM ALLOWANCES (9.5 FIELD ITEM ALLOWANCE - GENERAL* AL)

*NOT ALL WORK ITEMS ARE REPRESENTED BY A DRAWING DETAIL. REFER TO WORK ITEMS SCOPE AND TECHNICAL SPECIFICATIONS.

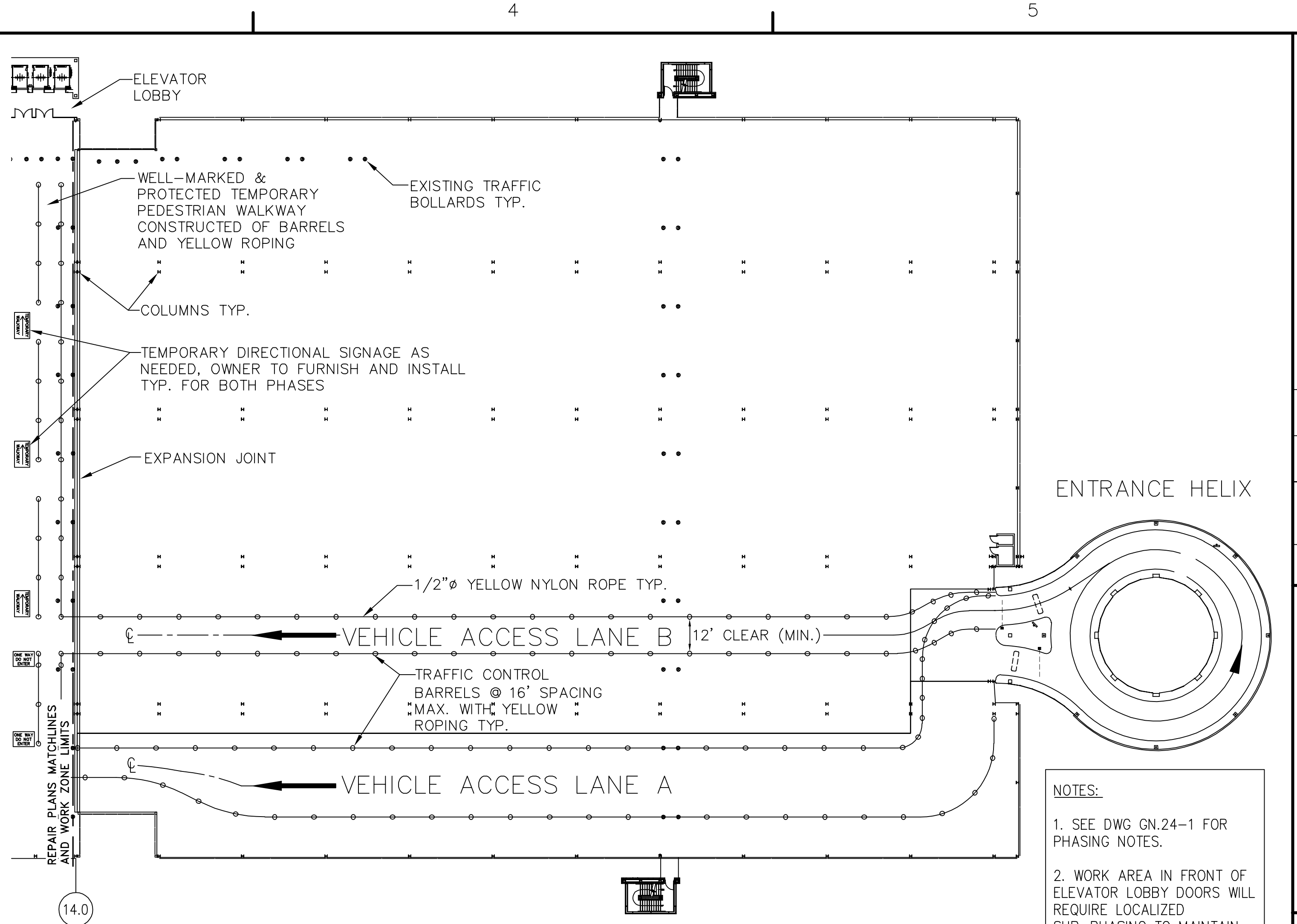
UNITS LEGEND: LS LUMP SUM, SF SQUARE FOOT, MD MAN-DAY(8 HR), LF LINEAR FEET, T&M TIME & MATERIAL, N/A NOT APPLICABLE, EA EACH, AL ALLOWANCE

Vertical sidebar containing: AECOM logo, project address (ACCOM PLAZA STREET, SUITE 401, MANCHESTER, NH 03101, (603) 606-4800), project name (CITY OF MANCHESTER - NEW HAMPSHIRE - DEPT. OF AVIATION, MANCHESTER - BOSTON REGIONAL AIRPORT, PARKING GARAGE, LEVEL-6 FLOOR & LEVEL-5 CEILING, SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS, F24-805-21), project number (60719585-103), drawing file (2_GN.24-1), designer (JGG), drawn by (CMP), dept check (MAB), proj check (RJD), date (APRIL 2024), scale (N/A), and sheet number (GN.24-1, SHEET 2 OF 13).

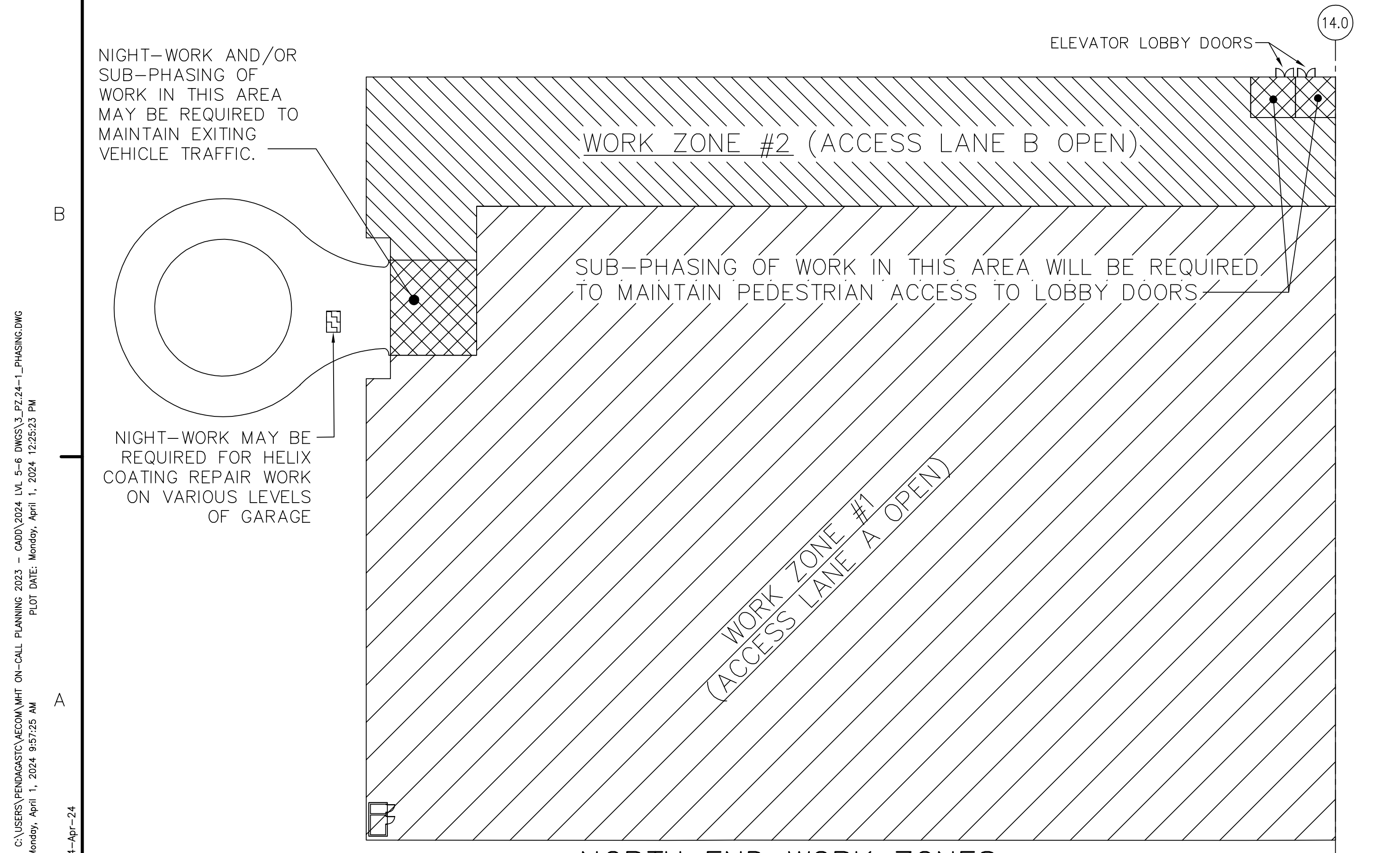
PATH/FILENAME: C:\USERS\PERDABUSHER\AECOM\WRT ON-CALL PLANNING 2023 - CAD\2024\LV 5-6 DWGS\2_GN.24-1 - GENNOTES.DWG, LAST UPDATE: Monday, April 1, 2024 9:51:37 AM, ARCH D - 14-Apr-24



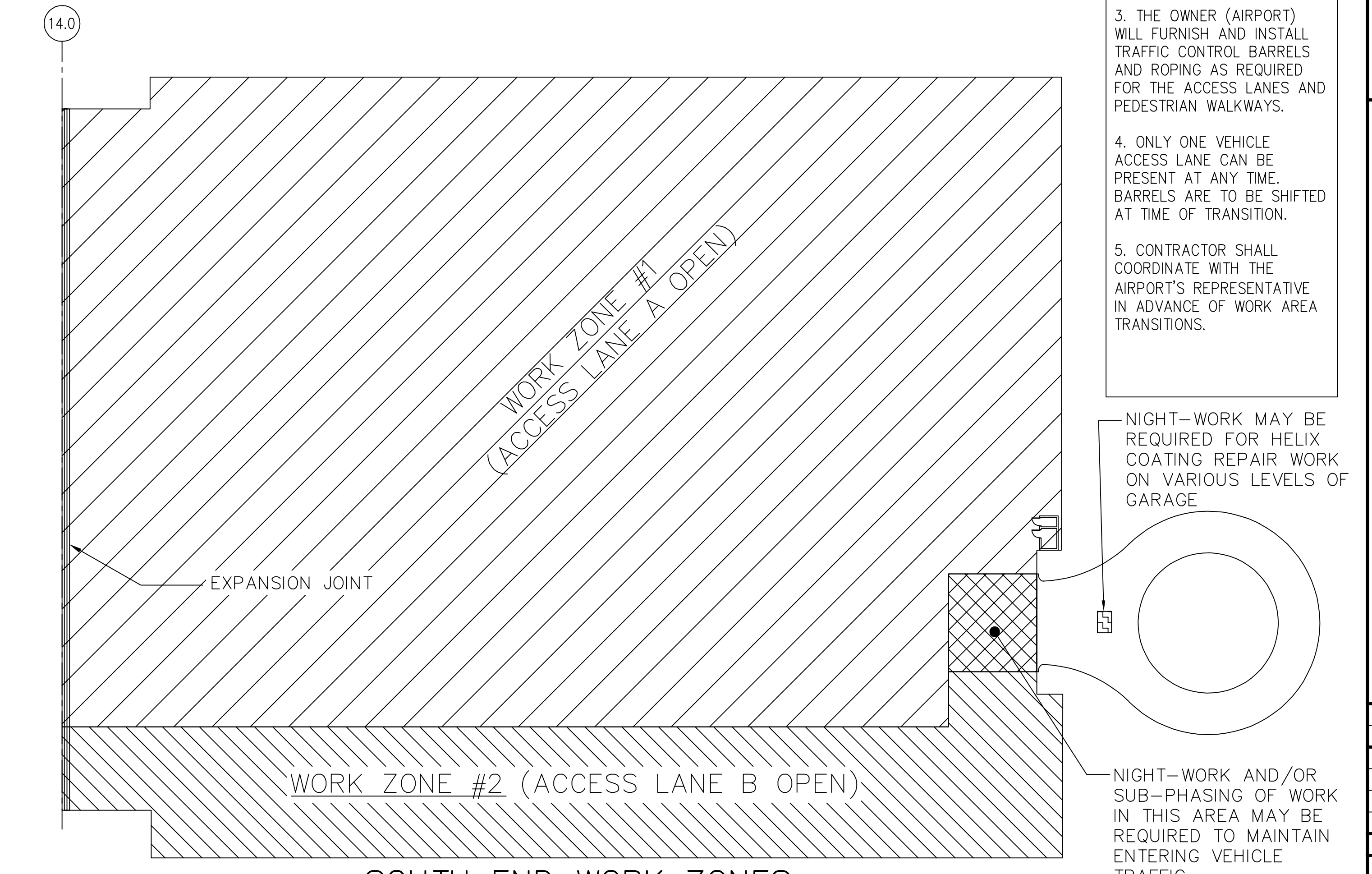
NORTH END TRAFFIC CONTROL



SOUTH END TRAFFIC CONTROL



NORTH END WORK ZONES
PHASE 6N-FLOOR / 5N-CEILING WORK AREA



SOUTH END WORK ZONES
PHASE 6S-FLOOR / 5S-CEILING WORK AREAS

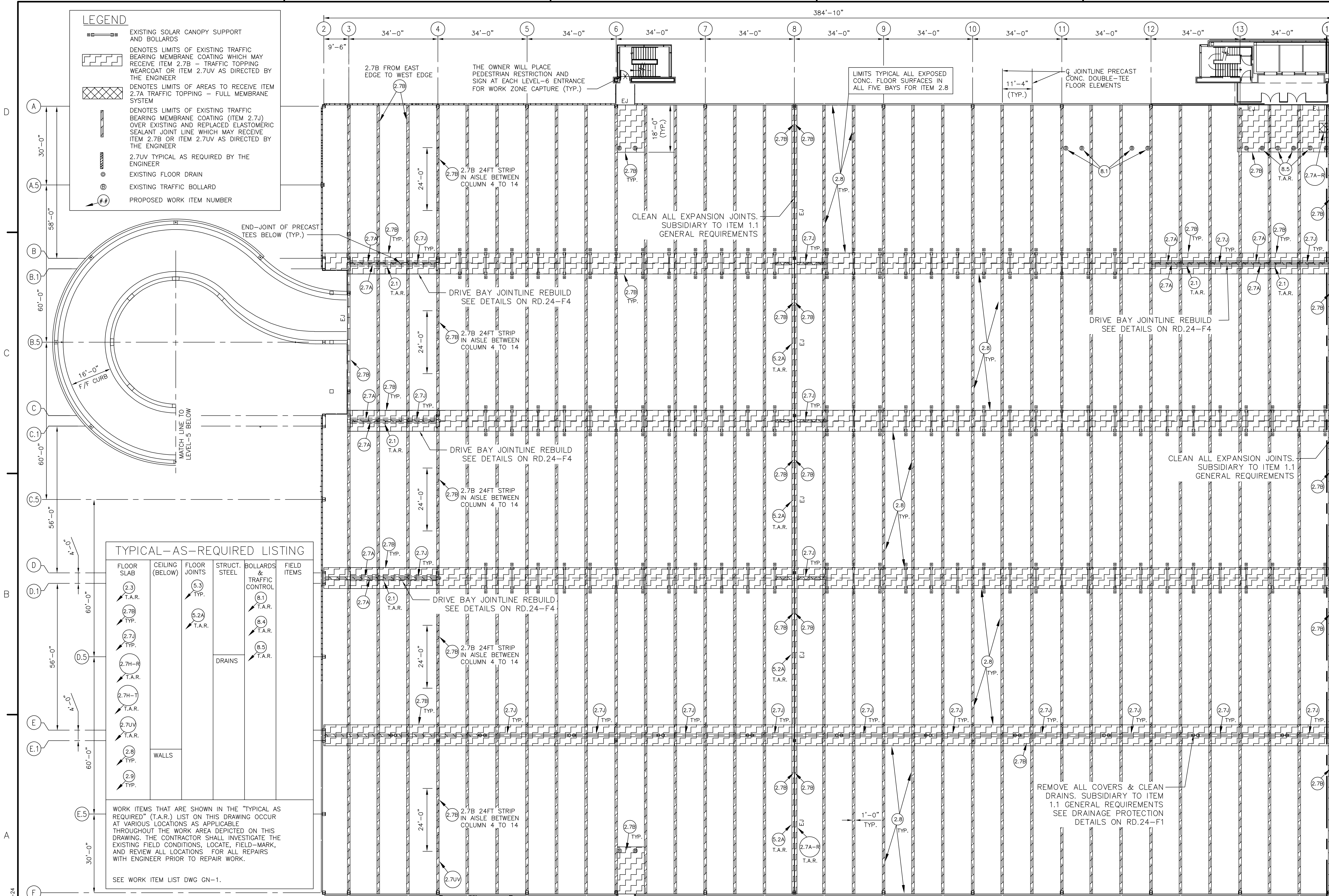
- NOTES:**
- SEE DWG GN.24-1 FOR PHASING NOTES.
 - WORK AREA IN FRONT OF ELEVATOR LOBBY DOORS WILL REQUIRE LOCALIZED SUB-PHASING TO MAINTAIN UNRESTRICTED PUBLIC ACCESS.
 - THE OWNER (AIRPORT) WILL FURNISH AND INSTALL TRAFFIC CONTROL BARRELS AND ROPING AS REQUIRED FOR THE ACCESS LANES AND PEDESTRIAN WALKWAYS.
 - ONLY ONE VEHICLE ACCESS LANE CAN BE PRESENT AT ANY TIME. BARRELS ARE TO BE SHIFTED AT TIME OF TRANSITION.
 - CONTRACTOR SHALL COORDINATE WITH THE AIRPORT'S REPRESENTATIVE IN ADVANCE OF WORK AREA TRANSITIONS.

PATH/FILENAME: C:\USERS\VENKATACR\AECOM\WRT_ON-CALL_PLANNING_2023 - CAD\2024\14_5-6_DWG\3_PZ24-1_PHASING.DWG
 LAST UPDATE: Monday, April 1, 2024 8:57:25 AM
 ARCH D - 14-Apr-24

<p>PROJECT NO: 60719585-T03 CAD DWG FILE: 3_PZ24-1 DESIGNED BY: JGG DRAWN BY: CMP DEPT CHECK: MAB PROJ CHECK: RJD DATE: APRIL 2024 SCALE: NOT TO SCALE</p>		<p>MARK</p> <p>DATE</p> <p>MADE BY</p> <p>CHECKED</p> <p>DESCRIPTION</p>
<p>CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION MANCHESTER - BOSTON REGIONAL AIRPORT PARKING GARAGE - LEVEL-6 FLOOR & LEVEL-5 CEILING SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS FY24-805-21</p>		<p>REVISIONS</p>
<p>AECOM 1555 ELM STREET, SUITE 401 MANCHESTER, NH 03101 (603) 686-4800</p>		<p>PHASING AND TRAFFIC PLAN: LEVEL 5 & 6</p>
<p>PZ.24-5/6</p>		<p>SHEET 3 OF 13</p>

LEGEND

- EXISTING SOLAR CANOPY SUPPORT AND BOLLARDS
- DENOTES LIMITS OF EXISTING TRAFFIC BEARING MEMBRANE COATING WHICH MAY RECEIVE ITEM 2.7B - TRAFFIC TOPPING WEARCOAT OR ITEM 2.7UV AS DIRECTED BY THE ENGINEER
- DENOTES LIMITS OF AREAS TO RECEIVE ITEM 2.7A TRAFFIC TOPPING - FULL MEMBRANE SYSTEM
- DENOTES LIMITS OF EXISTING TRAFFIC BEARING MEMBRANE COATING (ITEM 2.7J) OVER EXISTING AND REPLACED ELASTOMERIC SEALANT JOINT LINE WHICH MAY RECEIVE ITEM 2.7B OR ITEM 2.7UV AS DIRECTED BY THE ENGINEER
- 2.7UV TYPICAL AS REQUIRED BY THE ENGINEER
- EXISTING FLOOR DRAIN
- EXISTING TRAFFIC BOLLARD
- PROPOSED WORK ITEM NUMBER



TYPICAL-AS-REQUIRED LISTING

FLOOR SLAB	CEILING (BELOW)	FLOOR JOINTS	STRUCT. STEEL	BOLLARDS & TRAFFIC CONTROL	FIELD ITEMS
2.3 T.A.R.	5.3 TYP.			8.1 T.A.R.	
2.7B TYP.	5.2A T.A.R.			8.4 T.A.R.	
2.7J TYP.				8.5 T.A.R.	
2.7H-R T.A.R.					
2.7H-T T.A.R.					
2.7UV T.A.R.					
2.8 TYP.					
2.9 TYP.					

WORK ITEMS THAT ARE SHOWN IN THE "TYPICAL AS REQUIRED" LIST ON THIS DRAWING OCCUR AT VARIOUS LOCATIONS AS APPLICABLE THROUGHOUT THE WORK AREA DEPICTED ON THIS DRAWING. THE CONTRACTOR SHALL INVESTIGATE THE EXISTING FIELD CONDITIONS, LOCATE, FIELD-MARK, AND REVIEW ALL LOCATIONS FOR ALL REPAIRS WITH ENGINEER PRIOR TO REPAIR WORK.

SEE WORK ITEM LIST DWG GN-1.

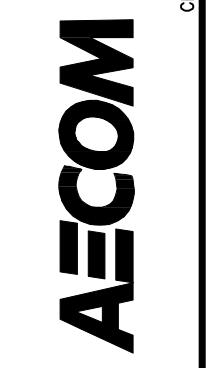


KEY PLAN NTS

- NOTES:
- CONTRACTOR SHALL ANNOTATE ALL REPAIR WORK PERFORMED ON THE RECORD STATUS DRAWINGS TO BE MAINTAINED ON THE SITE FOR SUBMITTAL TO THE ENGINEER AT THE COMPLETION OF EACH PHASE. SPECIFIC REPAIR ITEM NUMBER, LOCATION DIMENSIONS, SIZE/AREA OF REPAIR, AND OTHER PERTINENT INFORMATION SHALL BE NOTED ACCORDINGLY.
 - SEE PHASING DRAWINGS FOR WORK AREAS AND SAFETY AREA CAPTURE AND TRAFFIC CONTROL REQUIREMENTS.

WORK LIMITS PHASE 6N

1555 ELM STREET, SUITE 401
MANCHESTER, NH 03101
(603) 696-4800
CERTIFICATE OF AUTHORIZATION NO. 0000000000



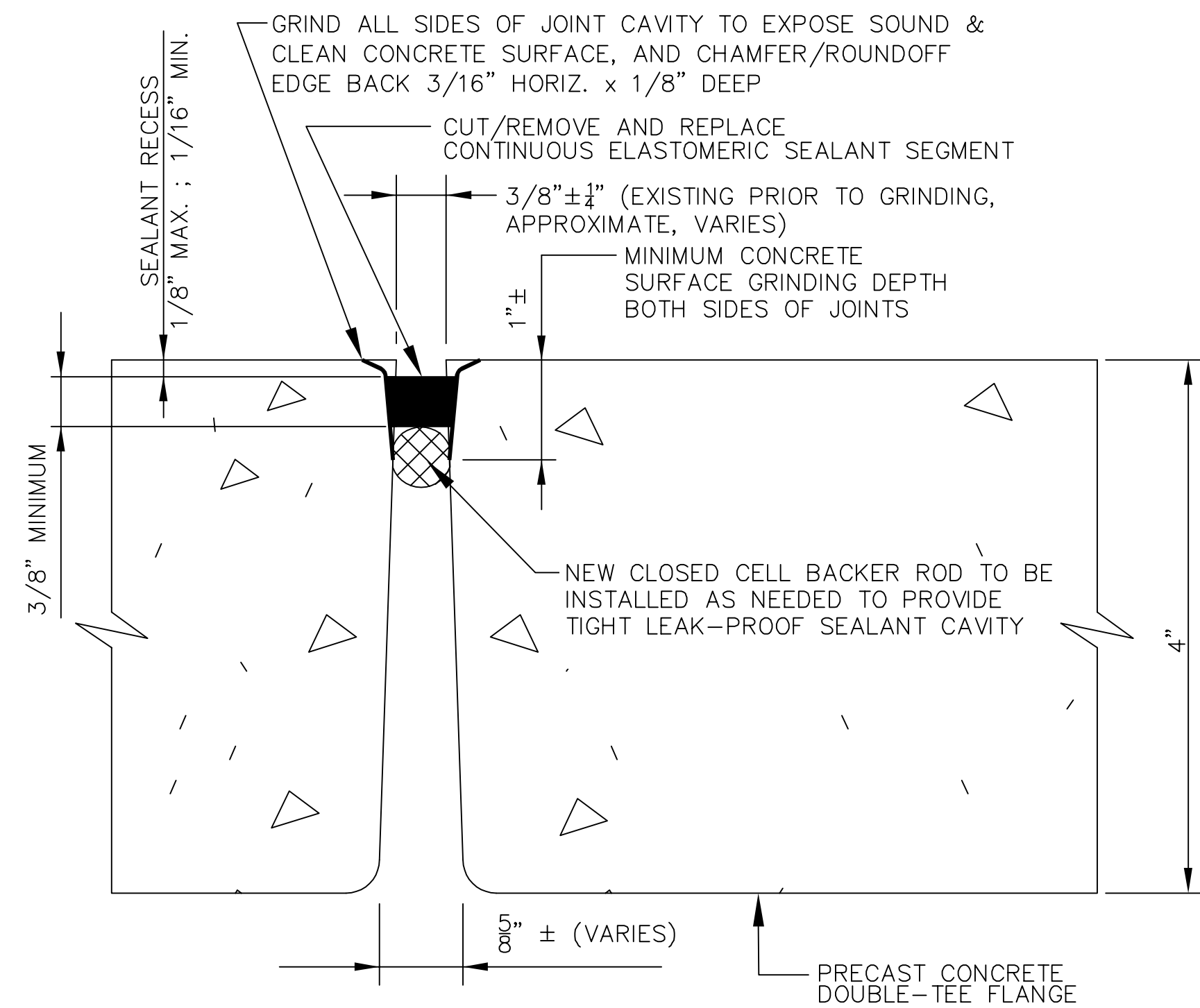
CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL-6 FLOOR & LEVEL-5 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY24-805-21

PROJECT NO:	60719585-103
CAD DWG FILE:	4-5_RP.24-6N
DESIGNED BY:	JGG
DRAWN BY:	CMP
DEPT CHECK:	JGG
PROJ CHECK:	JGG
DATE:	APRIL 2024
SCALE:	1/16" = 1'-0"
RP.24-FLR.6N	
SHEET 4 OF 13	

REVISIONS	DESCRIPTION	DATE	MADE BY	CHECKED BY

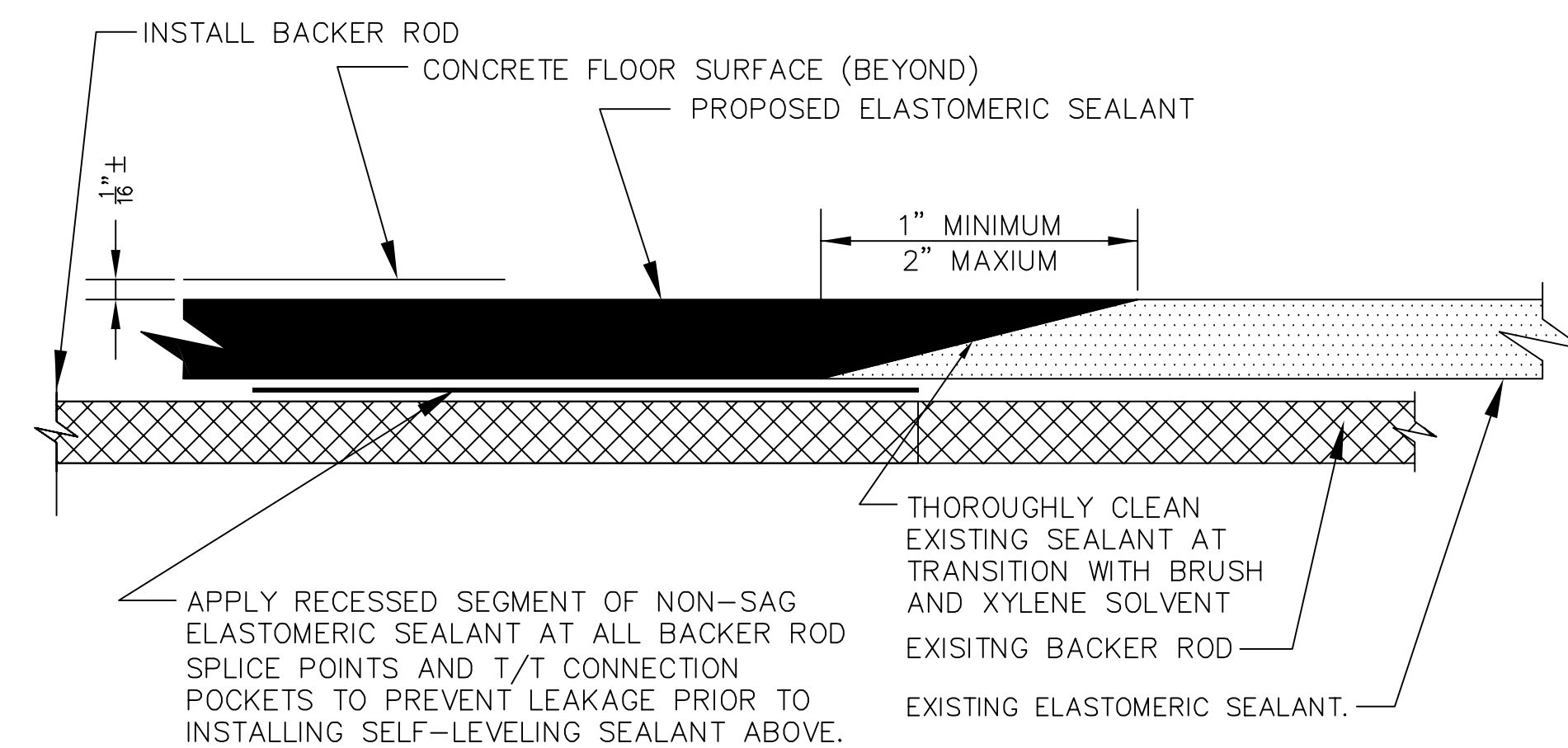
TEE-TEE FLANGE JOINT PREPARATION & SEALANT NOTES:

1. REMOVE EXISTING FAILED JOINT SEALANTS AND DEBONDED/CRACKED LOOSE CONCRETE TO SOUND CONCRETE WITHIN REPAIR AREAS.
2. IF THE DAMAGED CONCRETE REPAIR AREA EXTENDS MORE THAN 3/8" FROM THE FINISHED EDGE OF THE JOINT NOTIFY ENGINEER PRIOR TO PERFORMING REPAIRS.
3. GRIND CONCRETE SURFACES TO RECEIVE SEALANT APPLICATION. CLEAN JOINT THOROUGHLY INCLUDING BRUSH AND VACUUMING. SEALANT ADHESION SURFACES WHICH REMAIN OPEN AND ARE EXPOSED TO RAINWATER OR OTHER CONTAMINANTS SHALL BE DRIED THOROUGHLY AND WIPED CLEAN WITH SOLVENT (XYLENE OR APPROVED SEALANT MANUFACTURER RECOMMENDED ALTERNATE).
4. INSTALL CLOSED CELL BACKER ROD (AND NON-SAG ELASTOMERIC SEALANT AT BACKER ROD JOINTS) AS NEEDED TO ENSURE TIGHT LEAK-PROOF FIT. THE CONTRACTOR SHALL CLEAN ALL SEALANT LEAKS FROM THE UNDERSIDE CEILING SURFACES AND FLOOR BELOW.
5. PRIME ALL SURFACES TO RECEIVE SEALANT ADHESION WITH SEALANT MFR.'S RECOMMENDED PRIMER.
6. INSTALL SELF-LEVELING ELASTOMERIC SEALANT (SIKAFLEX 2C-SL, AS MFD. BY SIKA CORP OR APPROVED EQUAL) IN ACCORDANCE WITH MANUFACTURER'S WRITTEN INSTRUCTIONS. FINISHED SURFACE OF SEALANT SHALL BE 1/16" TO 1/8" (MAX.) BELOW TOP OF PRECAST CONCRETE FLOOR. DEPTH OF SEALANTS (3/8" MIN.) SHALL BE HALF OF THE JOINT WIDTH TO BE SEALED UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
7. EACH SEGMENT OF NEW SEALANT SHALL BE PLACED CONTINUOUSLY WITHOUT ALLOWING IN-PLACE SEALANTS TO SKIN-CURE BEFORE INSTALLING CONTINUATION OF THE SEALANT SEGMENT.
8. AFTER COMPLETION OF SEALANT REPLACEMENTS THE CONTRACTOR SHALL WATER TEST JOINTLINES TO VERIFY LEAK-TIGHT FLOOR JOINTS PRIOR TO SHOT-BLAST CONCRETE SURFACE PREPARATION AND TRAFFIC TOPPING MEMBRANE COATING SYSTEM.

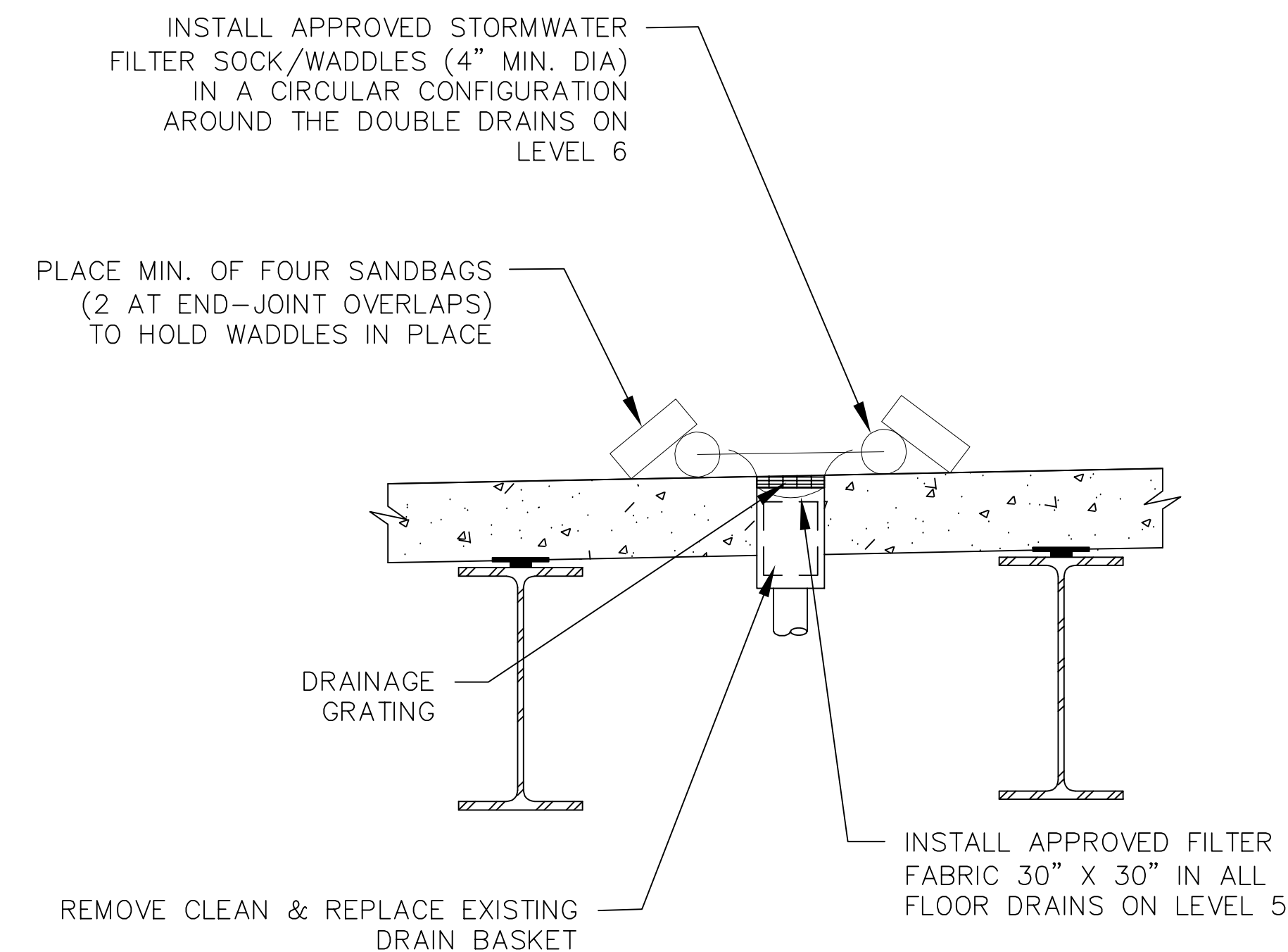


TYPICAL SECTION AT JOINT SEALANTS (BEYOND CONNECTION) SCALE: FULL

NOTE: CONTRACTOR SHALL VERIFY AND COORDINATE THAT THE EXTENTS OF ALL REPAIRS ARE WITHIN CAPTURED SAFETY AREAS ON THE LEVEL BELOW



SEALANT TRANSITION DETAIL (PROPOSED / EXISTING)

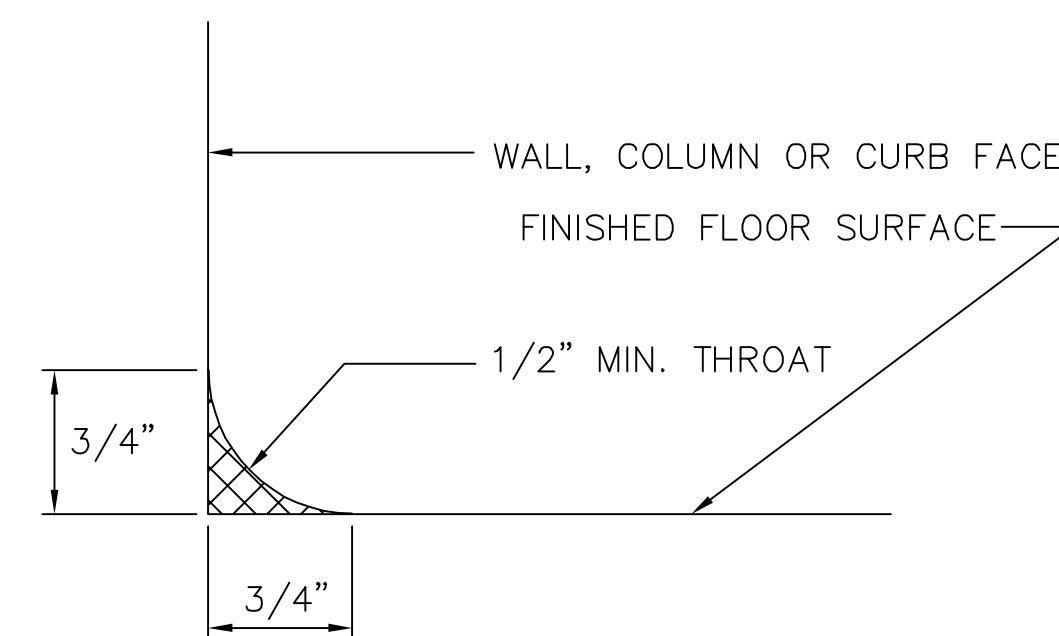


NOTES:

1. ALL DRAINAGE PROTECTION AND DRAIN CLEANING SHALL BE SUBSIDIARY TO ITEM 1.1 GENERAL CONDITIONS
2. CONTRACTOR SHALL CHECK, CLEAN AND REPLACE FILTER FABRIC PERIODICALLY TO MAINTAIN DRAINAGE FLOW
3. CONTRACTOR SHALL CLEAN THE COVER SEATS PRIOR TO REINSTALLATION OF DRAIN COVERS
4. OWNER WILL PROVIDE GRATES & DRAINAGE BASKETS AS NEEDED, CONTRACTOR WILL INSTALL SUBSIDIARY TO ITEM 1.1 GENERAL CONDITIONS

DRAINAGE PROTECTION SCALE: NTS

5.3 FLOOR SLAB JOINT REPAIR - REMOVE AND REPLACE T/T FLANGE JOINT SEALANTS SCALE: AS NOTED



NOTES (ITEM 5.6):

1. PREPARE CONCRETE SURFACES BY GRINDING AND OTHER SURFACES BY SOLVENT WIPE (XYLENE OR APPROVED EQUAL). APPLY PRIMER, AND ALLOW PRIMER TO CURE PER MANUF. RECOMMENDATIONS, PRIOR TO APPLYING SPECIFIED SEALANTS.
2. SEE SPECIFICATIONS FOR APPROVED MATERIALS.

5.6 FLOOR SLAB JOINT REPAIR - REMOVE AND INSTALL COVE SEALANT SCALE: NTS

LEGEND

#.# WORK ITEM NUMBER

NOTES (ITEM 5.2A):

WORK ITEM 5.2A NOSING SURFACE REPAIR: (LENGTH VARIES FROM 1'-0" TO 4'-0"±) SAWCUT MAXIMUM OF 3/8" DEEP AT LONGITUDINAL AND TRANSVERSE LIMITS OF NOSING MATERIAL TO BE REMOVED. USE CAUTION WHEN SAWCUTTING TO AVOID DAMAGE TO EXPANSION JOINT GLAND FLANGES.

EXISTING 1" WIDE ALUMINUM HOLD-DOWN STRIP WITH STEEL FASTENER & 1" DIA. STEEL WASHERS @ 12" C/C. REMOVE (GRIND-OFF) FASTENER HEAD AND WASHER.

INSTALL ELASTOMERIC CONCRETE NOSING MATERIAL

1" EXISTING NOSING MATERIAL BLOCKOUT

1 1/2" CONCRETE WASH

4"

7 1/2" EXISTING CONCRETE WASH

3" EXISTING NOSING BLOCKOUT

1.) SAWCUT & REMOVE EXISTING NOSING MATERIAL FROM DESIGNATED REPAIR AREA TO SOUND SUBSTRATE MATERIAL (TOP OF EXPANSION JOINT GLAND FLANGE & HOLD-DOWN STRIP).

2.) REMOVE HOLD-DOWN FASTENER HEADS & WASHERS WITHIN THE REPAIR AREA BY GRINDING. HOLD-DOWN STRIP TO REMAIN. INSTALL NEW STAINLESS STEEL FASTENERS AS REQUIRED TO HOLD THE EXPANSION JOINT GLAND FLANGE IN PLACE AND KEEP THE ALUMINUM STRIP FIRMLY AT THE BOTTOM OF THE NOSING MATERIAL.

3.) PREPARE EXPOSED SURFACES FOR ELASTOMERIC CONCRETE NOSING MATERIAL BY SANDBLASTING OR MECHANICAL MEANS ACCORDING TO MANUFACTURER'S RECOMMENDATIONS.

4.) INSTALL ELASTOMERIC CONCRETE NOSING MATERIAL IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.

LIMITS OF WORK ITEM 2.7A TO BE APPLIED AT BOTH SIDES OF EXPANSION JOINTS AS DIRECTED BY THE ENGINEER. TERMINATE COATING EDGE IN 1/2" DEEP SAWCUT.

PRECAST CONCRETE DOUBLE TEE FLANGE (TYP.) EXISTING EXPANSION JOINT GLAND (TO REMAIN). TREMCO DEKITE EXP. JOINT SYSTEM; MODEL #525 NTS. 2" EXP. JOINT AT LINES 8 & 20

5.2A FLOOR SLAB JOINT REPAIR - EXPANSION JOINT NOSING-SURFACE REPAIR SCALE: NTS

1555 ELM STREET, SUITE 401
MANCHESTER, NH 03101
(603) 696-4800

AECOM

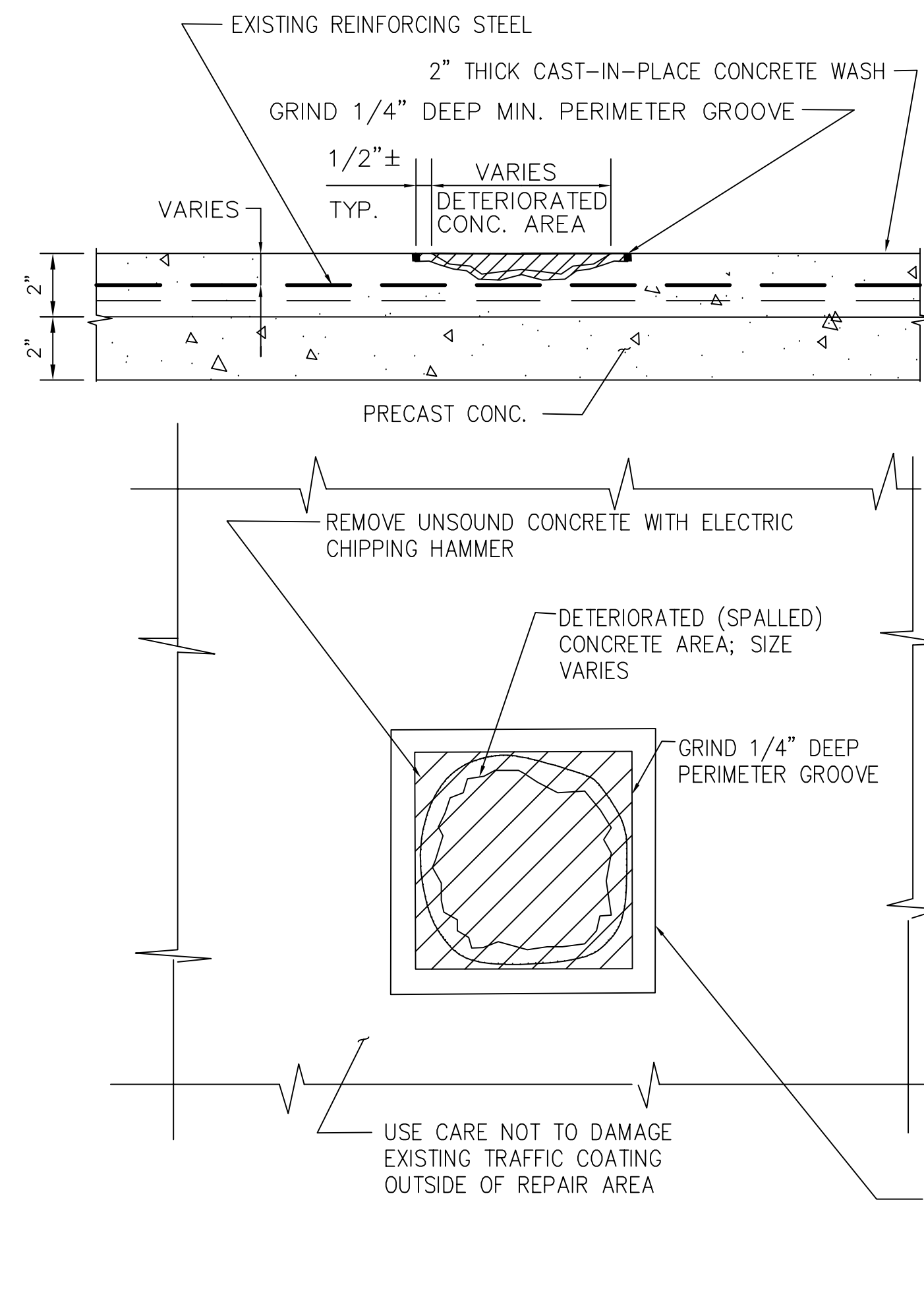
CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL-6 FLOOR & LEVEL-5 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY24-805-21

FLOOR REPAIR DETAILS - I

PROJECT NO:	60719585-T03
CAD DWG FILE:	8-10_RD.24-1-3
DESIGNED BY:	JGG
DRAWN BY:	CMP
DEPT CHECK:	MAB
PROJ CHECK:	RJD
DATE:	APRIL 2024
SCALE:	AS NOTED

NOTES (ITEM 2.2):

1. THE CONTRACTOR SHALL CHAIN DRAG THE FULL SURFACE OF THE CIP CONC. WASH AREAS (6FT WIDE X FULL COLUMN ROW LENGTH) AND MARK LOCATIONS & EXTENTS OF SPALLED/DETERIORATED CONC. THEN REVIEW EACH AREA WITH THE ENGINEER FOR APPROVAL PRIOR TO BEGINNING DEMOLITION SUBSIDIARY TO THE OVERALL WORK UNDER ITEM 1.1 GENERAL REQUIREMENTS.
2. GRIND A PERIMETER GROOVE 1/4" DEEP AROUND EXTENTS OF REPAIR AREA ENSURING THAT ALL EDGES ARE ENCOMPASSED BY THE GROOVE. THE PERIMETER GROOVE SHOULD BE CONTINUOUS AND CONSISTANT DEFINING A CONCRETE REPAIR AREA THAT CLOSELY FOLLOWS THE GENERAL SHAPE OF THE DETERIORATION AREA IN A NEAT WORKMANSHIP MANNER WITH A UNIFORM PATTERN AND NO SHARP ANGLES.
3. REMOVE DETERIORATED CONCRETE IN REPAIR AREA TO SOUND CONCRETE BY CHIPPING WITH HAND-HELD LIGHT-WEIGHT CHIPPING HAMMERS AND CARE MUST BE TAKEN TO ENSURE CONCRETE REMOVAL DOES NOT CREATE CRACKING OR FRACTURES BEYOND THE ORIGINAL DETERIORATED CONCRETE REMOVAL AREA OR IN THE PRECAST CONC. BELOW.
4. REMOVE REMAINING CONCRETE WITHIN THE PERIMETER GROOVE LINES TO A DEPTH GRADUALLY TAPERING FROM THE 1/4" DEPTH AT THE GROOVE TO THE EXTENTS OF THE SOUND CONCRETE IN THE DEEPEST PART OF THE REPAIR AREA (VARIES). EXPOSED AGGREGATE SURFACE WITH A SURFACE PROFILE OF 1/16"± SHOULD REMAIN.
5. CLEAN ALL EXPOSED REINFORCING STEEL AND CONCRETE SURFACES BY MEANS OF BLAST CLEANING W/ ABRASIVE MEDIA TO NEAR-WHITE METAL SURFACE SSPC-SP10 (SUBSIDIARY) TO REMOVE ALL LOOSE MATERIAL, RUST, AND LATENCE. CONTRACTOR SHALL TAKE CARE TO NOT DAMAGE EXISTING REINFORCING BARS. REINFORCING BARS DAMAGED BY THE CONTRACTOR'S CONSTRUCTION PRACTICES SHALL BE EXPOSED AND SPLICED IN A MANNER AS DIRECTED BY THE ENGINEER AT NO COST TO THE OWNER.
6. COAT ALL REINFORCING STEEL AND CONCRETE SURFACES WITH THE REPAIR MORTAR MANUFACTURER'S APPROVED BONDING AGENT/PROTECTIVE COATING SUBSIDIARY TO ITEM 2.2 (EMACO P24 BONDING AGENT, SIKA ARMATEC 110 EPOCEM, OR APPROVED EQUIV.).
7. INSTALL POLYMER-MODIFIED CEMENTITIOUS REPAIR MORTAR (NEAT) WITH INTEGRAL PENETRATING CORROSION INHIBITOR (EMACO R310-CI AS MFD BY DEGUSSA/MASTER BUILDERS TECHNOLOGIES, SIKATOP 122 PLUS AS MFD. BY SIKA CORP., OR APPROVED EQUIV.) IN ACCORDANCE WITH MFR. INSTRUCTIONS. SCRUB-COAT, TROWEL, AND BROOM FINISH TO MATCH ADJOINING SURFACES.

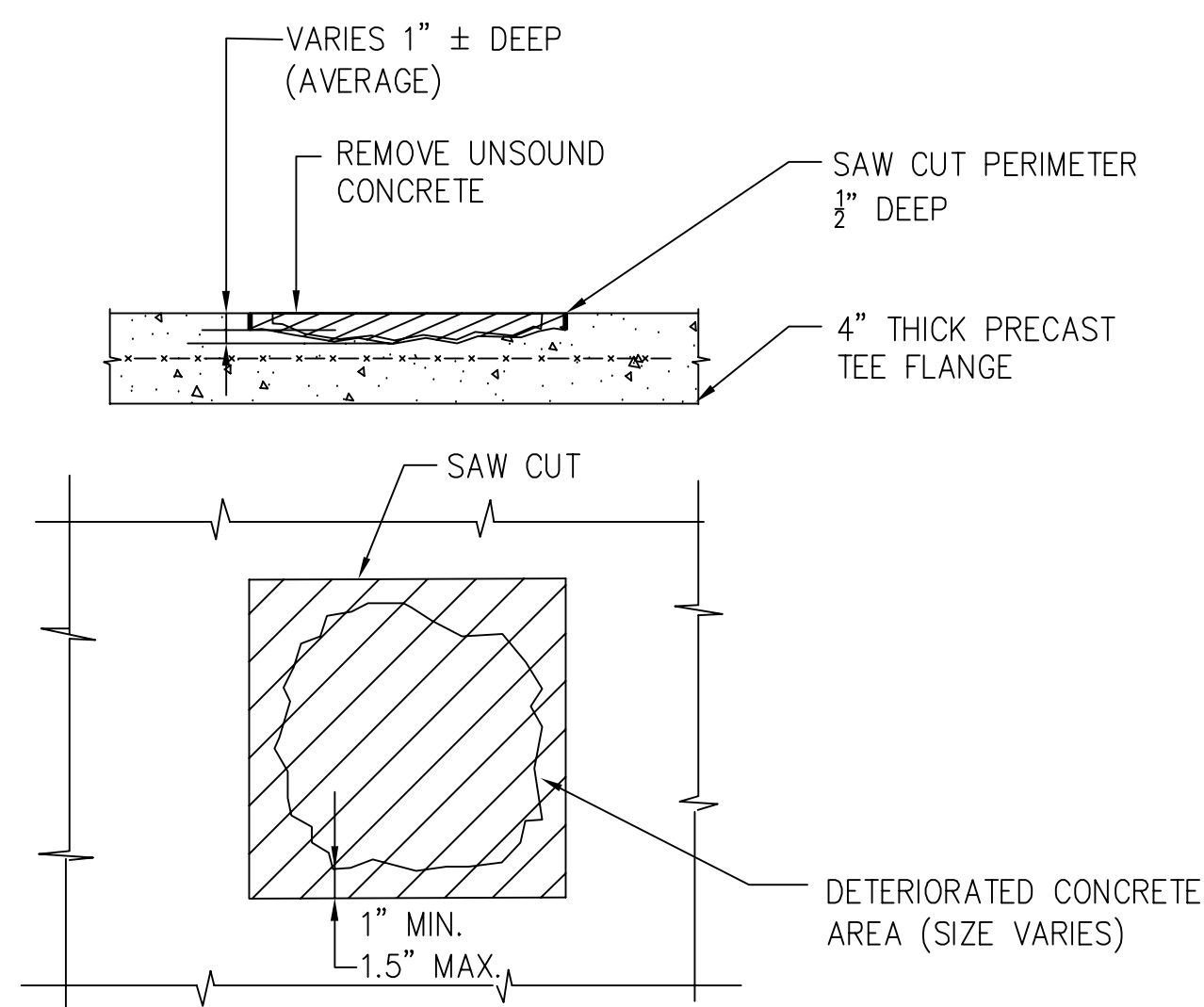


FLOOR REPAIR – PARTIAL DEPTH AT C.I.P. CONCRETE WASH

2.2 SCALE: 3" = 1'-0"

NOTES (ITEM 2.3):

1. LOCATE AND MARK AREAS FOR REPAIR WITH THE ENGINEER, AND CAPTURE AN ADEQUATE SAFETY AREA ON THE FLOOR LEVEL BELOW THE REPAIR AREA WITH CONSTRUCTION BARRELS AND ROPE.
2. REMOVE DETERIORATED CONCRETE OR OLD PATCHING MATERIALS TO SOUND CONCRETE (15-LB CLASS HAMMERS MAX.)
3. SAW CUT PERIMETER OF CONCRETE REPAIR AREA IN A NEAT WORKMANSHIP MANNER WITH A UNIFORM PATTERN; REMOVE REMAINING CONCRETE WITHIN SAW CUT LINES.
4. CLEAN ALL EXPOSED REINF. BARS AND CONCRETE BY SANDBLAST (SUBSIDIARY). PROVIDE (SUBSIDIARY) SUPPLEMENTARY REINFORCING BARS OR WELDED WIRE FABRIC (WWF) AS REQUIRED BY DETAILS OR AS DIRECTED BY THE ENGINEER.
5. COAT ALL REINFORCING AND CONCRETE SURFACES WITH EPOXY BONDING AGENT/PROTECTIVE COATING SUBSIDIARY TO THIS ITEM (MASTER EMACO P 124 , SIKA ARMATEC 110 EPOCEM, OR APPROVED EQUAL.)
6. APPLY CEMENTITIOUS REPAIR MORTAR IN ACCORDANCE WITH SECTION 03732 CONCRETE REPAIR, AND HEAVY BROOM FINISH TO MATCH ADJOINING SURFACES.
7. ENSURE LOCALIZED SAFETY AREA BELOW DEMOLITION AREA IS CLEAR AND CAPTURED. SAFETY WATCH/MONITORING OF SAFETY AREA BELOW DURING CHIPPING MAY BE REQUIRED AS DIRECTED BY THE ENGINEER



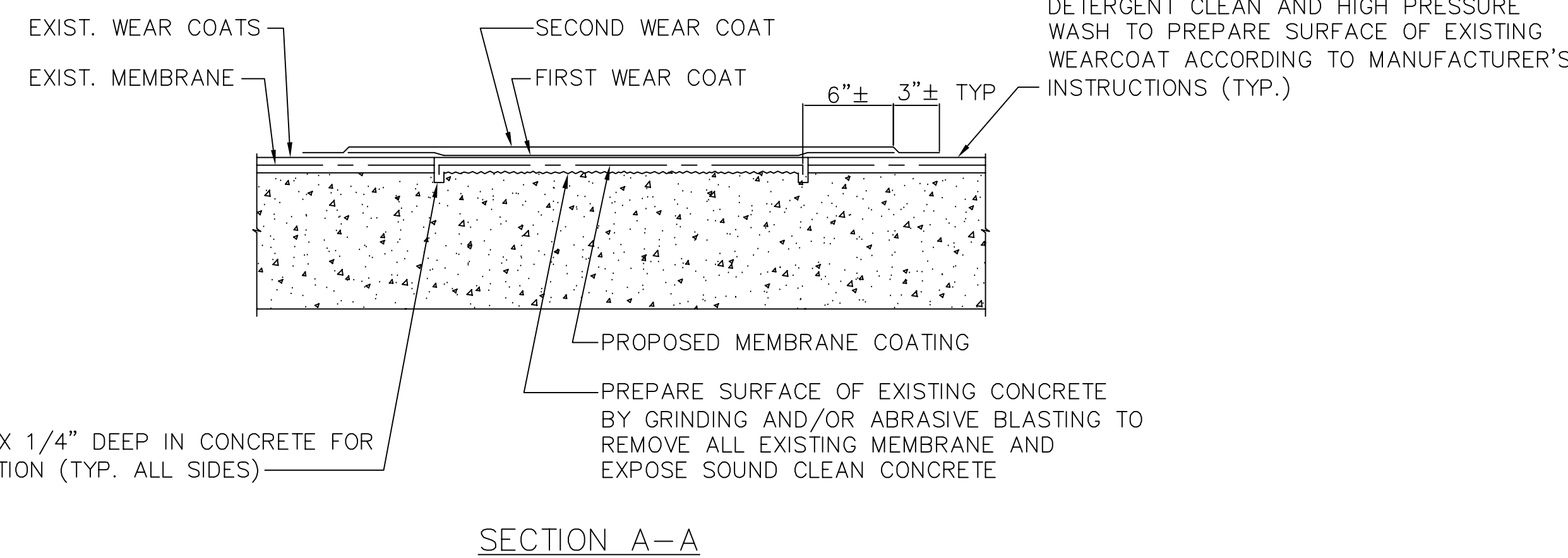
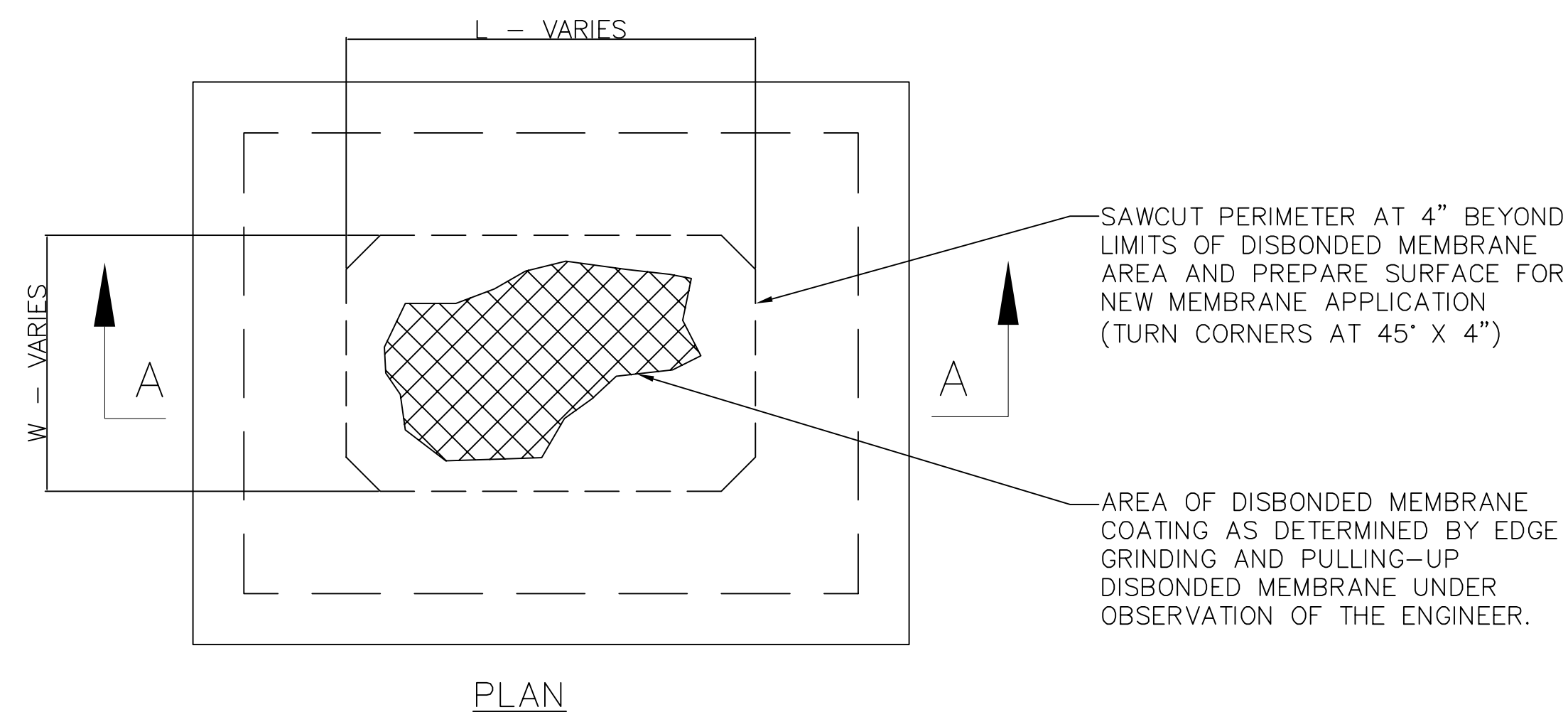
FLOOR REPAIR – PARTIAL DEPTH AT PRECAST CONCRETE FLOOR SLAB

2.3 SCALE: 1 1/2" = 1'-0"

LEGEND

WORK ITEM NUMBER

DETERIORATED CONCRETE REMOVAL



NOTES (ITEM 2.7H-R):

1. IT IS ANTICIPATED THAT THE LOCATIONS OF THE REPAIR AREAS MAY NOT ALLOW HELIX TRAFFIC TO BE MAINTAINED AND TO PASS SAFELY AROUND THE REPAIR AREAS WITH PROPER TRAFFIC CONTROL DEVICES AS REQUIRED. PREPARATIONS AND APPLICATION OF COATINGS FOR ITEM 2.7H-R SHALL BE PERFORMED DURING NIGHT-TURN HOURS (12:00PM TO 5:00AM) IF IT IS DETERMINED THAT THE LOCATION OF A REPAIR AREA PREVENTS TRAFFIC PASSAGE IN THE HELIX.
2. REPAIR AREAS SHALL BE PROTECTED FROM TRAFFIC WITH BARRELS, OR CONES AS REQUIRED AT ALL TIMES UNTIL COMPLETE.
3. MEASUREMENT FOR PAYMENT OF ITEM 2.7H-R SHALL BE MADE AFTER THE SAWCUT LINES ARE COMPLETE AND PRIOR TO COATING APPLICATION AS L X W.

2.7H-R FLOOR REPAIR –TRAFFIC TOPPING MEMBRANE REPAIR AT ENTRANCE & EXIT HELIX SCALE: NTS

2.7A-R FLOOR REPAIR –TRAFFIC TOPPING – FULL MEMBRANE SYSTEM REPAIR AREA SCALE: NTS

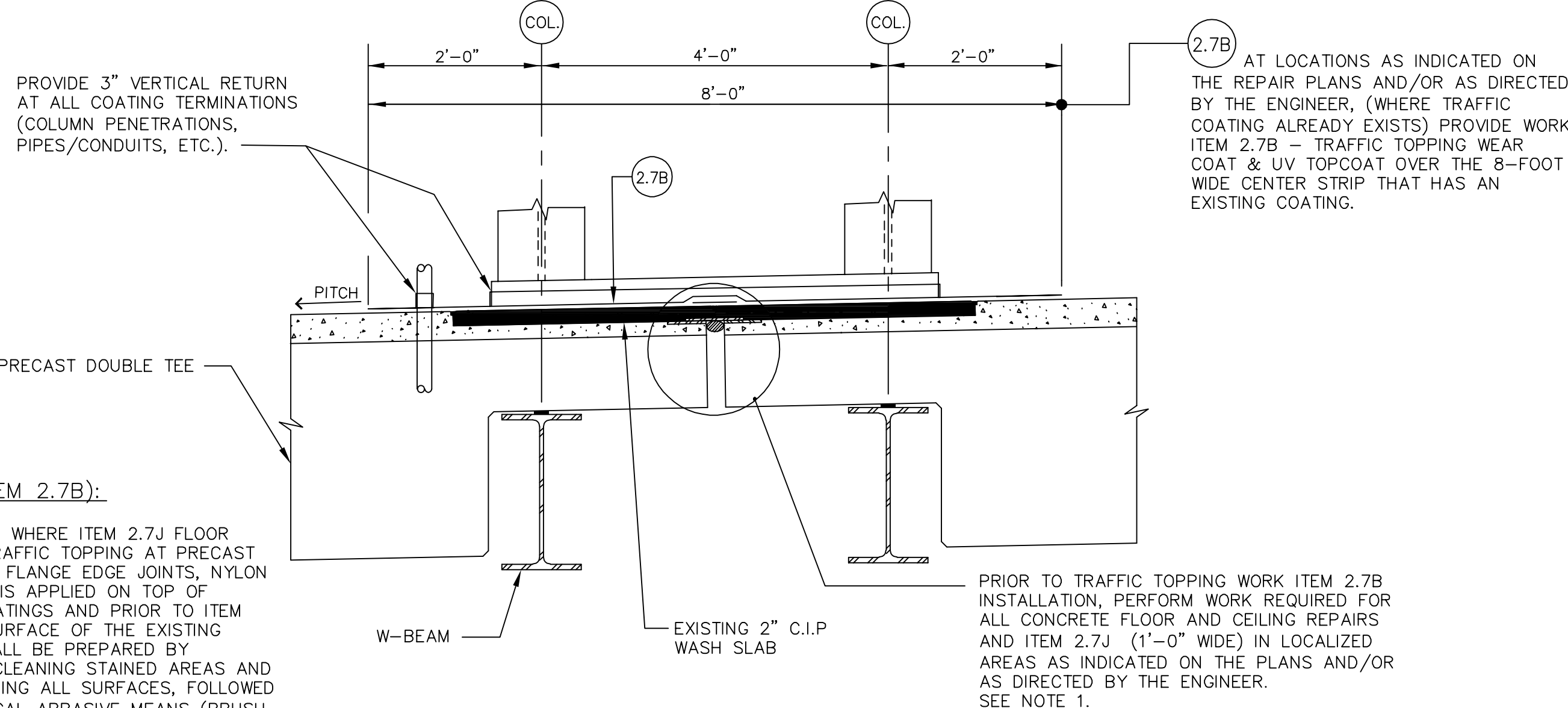
KEY: 1155 ELM STREET, SUITE 401 MANCHESTER, NH 03101 (603) 696-4800



CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE: LEVEL-6 FLOOR & LEVEL-5 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY24-805-21

FLOOR REPAIR DETAILS - II

PROJECT NO:	60719585-T03
CAD DWG FILE:	8-10_RD.24-1-3
DESIGNED BY:	JGG
DRAWN BY:	CMP
DEPT CHECK:	MAB
PROJ CHECK:	RJD
DATE:	APRIL 2024
SCALE:	AS NOTED



NOTES (ITEM 2.7B):

- 1.) IN AREAS WHERE ITEM 2.7J FLOOR REPAIR - TRAFFIC TOPPING AT PRECAST TEE-TO-TEE FLANGE JOINTS, NYLON REINFORCED IS APPLIED ON TOP OF EXISTING COATINGS AND PRIOR TO ITEM 2.7B, THE SURFACE OF THE EXISTING COATING SHALL BE PREPARED BY DETERGENT CLEANING STAINED AREAS AND POWER WASHING ALL SURFACES, FOLLOWED BY MECHANICAL ABRASIVE MEANS (BRUSH BLAST WITH ABRASIVE MEDIA) AND WIPED WITH XYLENE (OR APPROVED EQUAL) SOLVENT, IN ACCORDANCE WITH THE COATING MANUFACTURER'S RECOMMENDATIONS.
- 2.) THE PAY QUANTITY AREA FOR ITEM 2.7B WILL BE REDUCED FOR THE EQUIVALENT AREA OF ITEM 2.7J APPLIED WITHIN THE ITEM 2.7B LIMITS AS DIRECTED.

FLOOR REPAIR - TRAFFIC TOPPING - WEARCOAT AND UV-TOPCOAT

2.7B SCALE: 3/4" = 1'-0"

PRIOR TO TRAFFIC TOPPING WORK ITEM 2.7B INSTALLATION, PERFORM WORK REQUIRED FOR ALL CONCRETE FLOOR AND CEILING REPAIRS AND ITEM 2.7J (1'-0" WIDE) IN LOCALIZED AREAS AS INDICATED ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER. SEE NOTE 1.

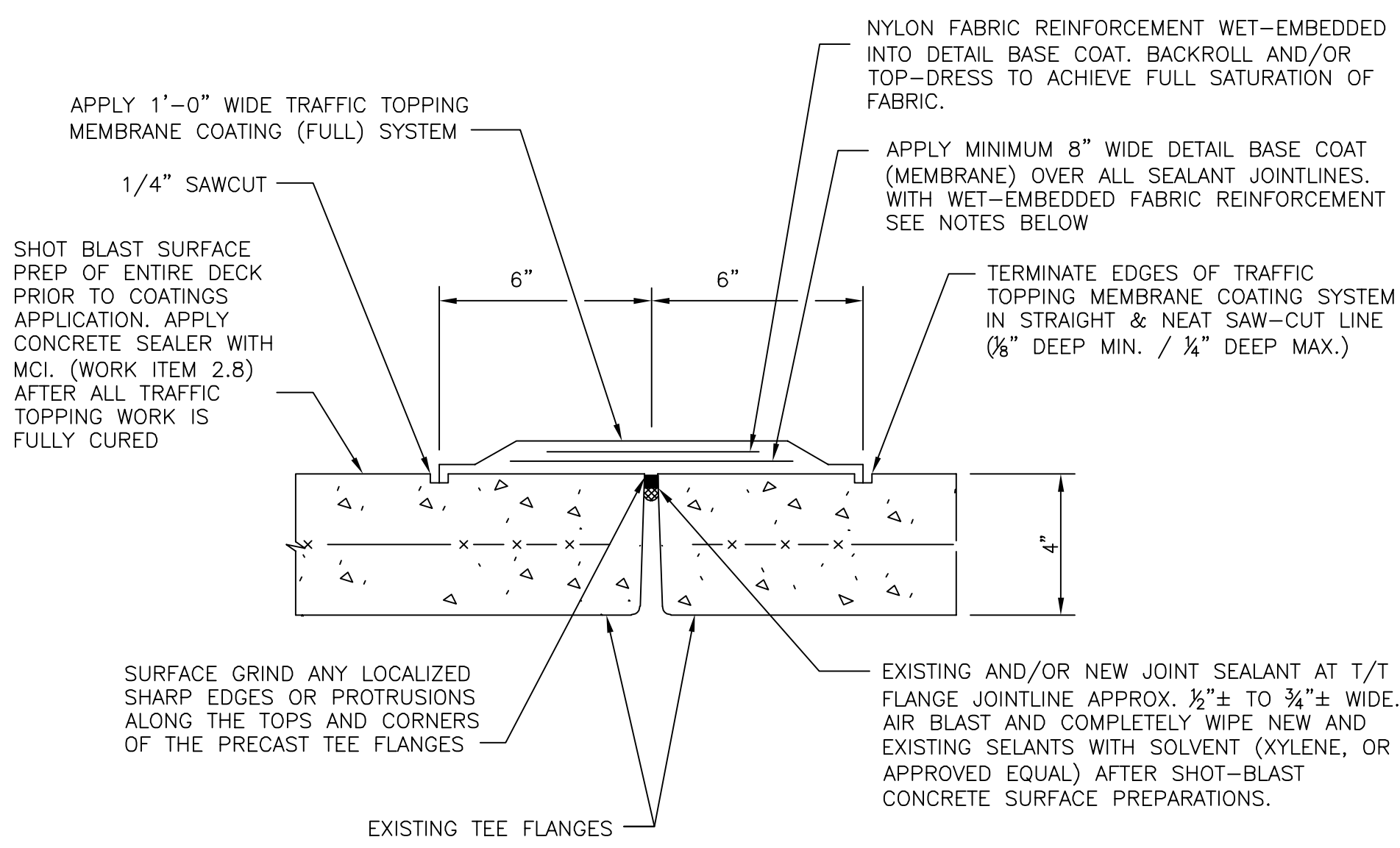
2.7B AT LOCATIONS AS INDICATED ON THE REPAIR PLANS AND/OR AS DIRECTED BY THE ENGINEER, (WHERE TRAFFIC COATING ALREADY EXISTS) PROVIDE WORK ITEM 2.7B - TRAFFIC TOPPING WEAR COAT & UV TOPCOAT OVER THE 8-FOOT WIDE CENTER STRIP THAT HAS AN EXISTING COATING.

LEGEND

#.#	WORK ITEM NUMBER
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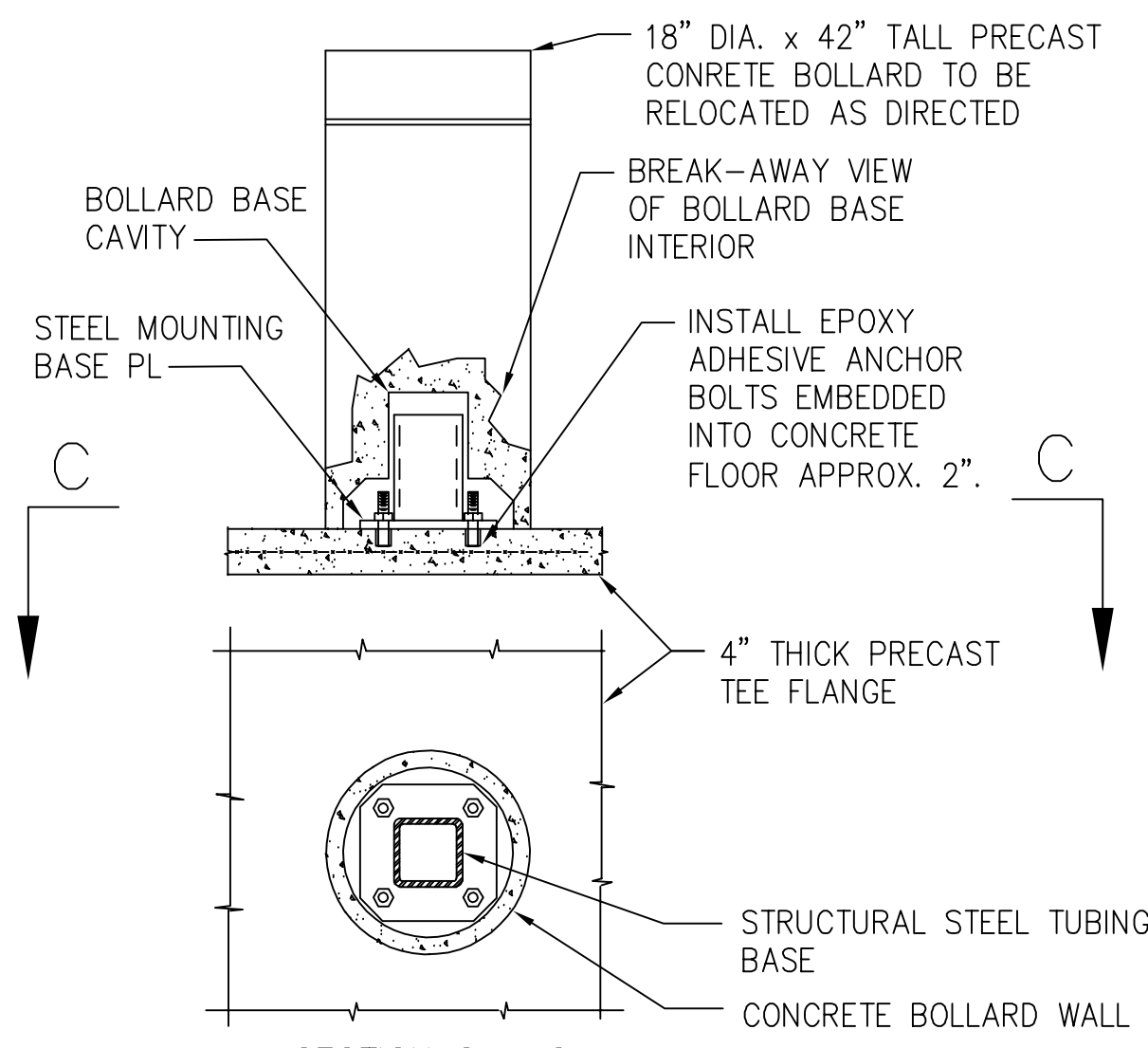
NOTES (ITEM 2.7J):

1. RECESSES, DEPRESSIONS, OR IRREGULARITIES ALONG THE JOINTLINES SHALL BE FILLED TO PROVIDE A SMOOTH LEVEL COATING TRANSITION BETWEEN PRECAST TEE FLANGES ACROSS THE JOINTLINES. PROVIDE A 100% SOLIDS FLEXIBLE EPOXY FILLER PASTE MATERIAL AS NEEDED PRIOR TO THE DETAIL BASE COAT -OR- THE DETAIL BASE COAT APPLICATION SHALL BE APPLIED IN THICKNESS AS REQUIRED ACROSS THE JOINTLINES TO FILL THE JOINTLINE AND PROVIDE THE SMOOTH TRANSITION (IN ACCORDANCE WITH THE COATING SYSTEM MANUFACTURER RECOMMENDATIONS).
2. A 6" NYLON FABRIC REINFORCEMENT STRIP SHALL BE WET-EMBEDDED AND BACKROLLED INTO THE DETAIL BASE COAT TO BE SUPPLIED BY THE COATING SYSTEM MANUFACTURER AS PART OF THE COATING SYSTEM TO PROVIDE JOINT-BRIDGING STRENGTH FOR A 5-YEAR WARRANTY OF THE COATING SYSTEM FOR THIS APPLICATION.
3. SEE SPECIFICATIONS FOR APPROVED MATERIALS.
4. ALL COATING EDGES SHALL BE MASKED/TAPED OR OTHERWISE GUARDED TO ENSURE NEAT, STRAIGHT, AND TRUE LINES. MASKING SHALL BE REMOVED PRIOR TO COATING COMPONENT CURE AS REQUIRED TO PREVENT DAMAGE OR MASKING TAPE RESIDUE. EXCESS COATING OR TAPE RESIDUE SHALL BE REMOVED BY GRINDING AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST TO THE OWNER.
5. WHERE EXISTING ITEM 2.7J MUST BE REMOVED MAKE EDGE CUTS (SAW OR GRINDER) AND REMOVE FULL COATING SYSTEM DOWN TO CONCRETE BY MECHANICAL MEANS (GRINDER WHEEL, ABRASIVE BLAST, SCABBLER, SHOT BLAST, OR HYDRO-REMOVAL) TO CLEAN CONCRETE SURFACE OR SOUND SUBSTRATE AS APPROVED BY THE ENGINEER SUBSIDIARY TO THE WORK UNDER ITEM 2.7J AND/OR ITEM 1.1.



FLOOR REPAIR - NYLON REINFORCED TRAFFIC TOPPING (1FT WIDE) AT JOINTLINE PRECAST TEE-TO-TEE FLANGE

2.7J SCALE: 3"=1'-0"

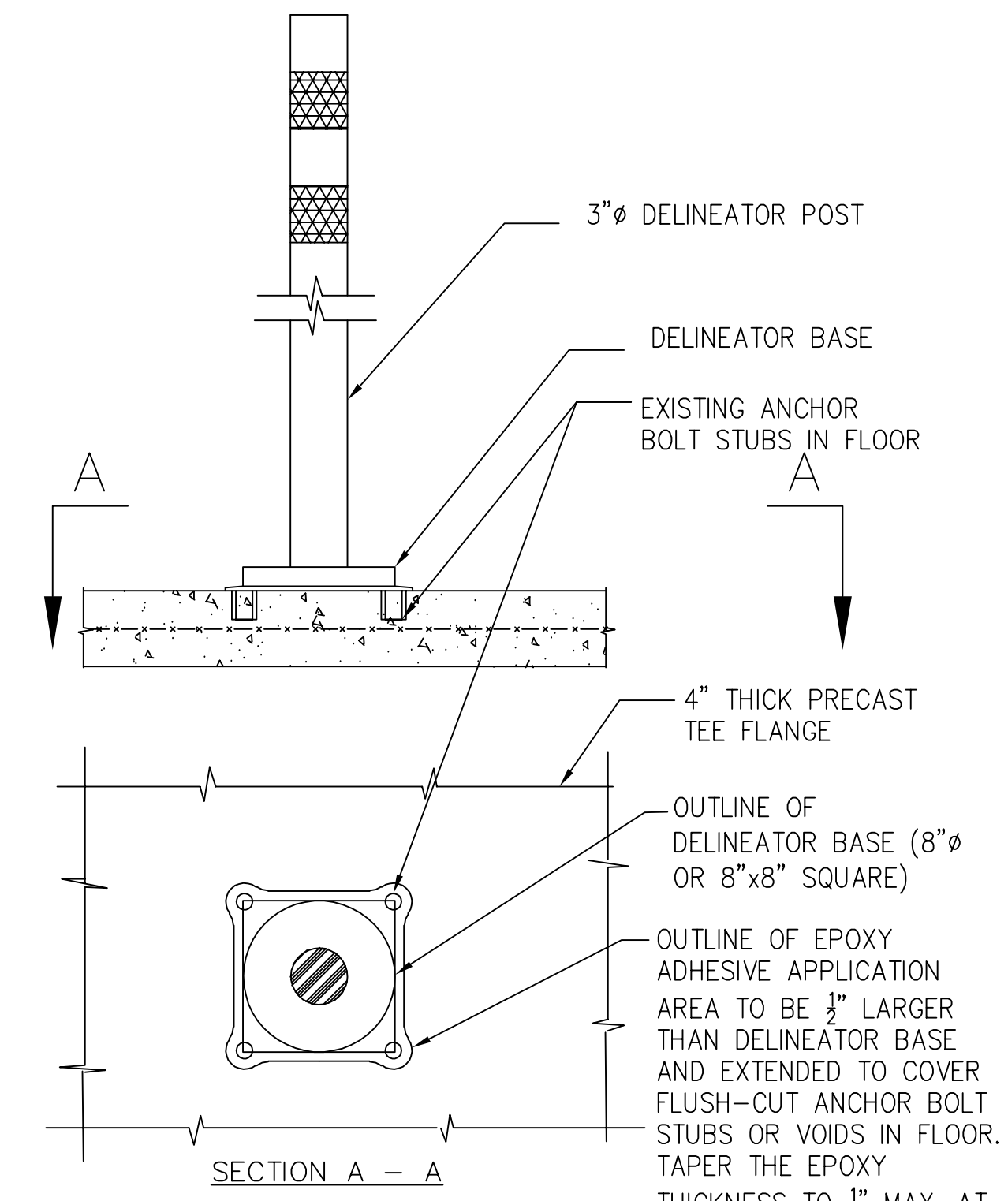


NOTES (ITEM 8.5):

1. DRILL AND EPOXY FOUR 5" LONG x 3/4" DIA. x 2" DEEP STAINLESS STEEL ALL THREADED BOLTS INTO DECK TO MATCH EXISTING ANCHORAGE.
2. PERFORM FLOOR REPAIRS AND INSTALL TRAFFIC COATINGS AS DIRECTED PRIOR TO INSTALLING BOLLARDS.
3. CLEAN AND COAT SALVAGED STEEL BASE WITH COLD-GALV PROTECTIVE COATING

BOLLARDS & TRAFFIC CONTROL - REINSTALL SALVAGED CONCRETE BOLLARD

8.5 SCALE: NTS

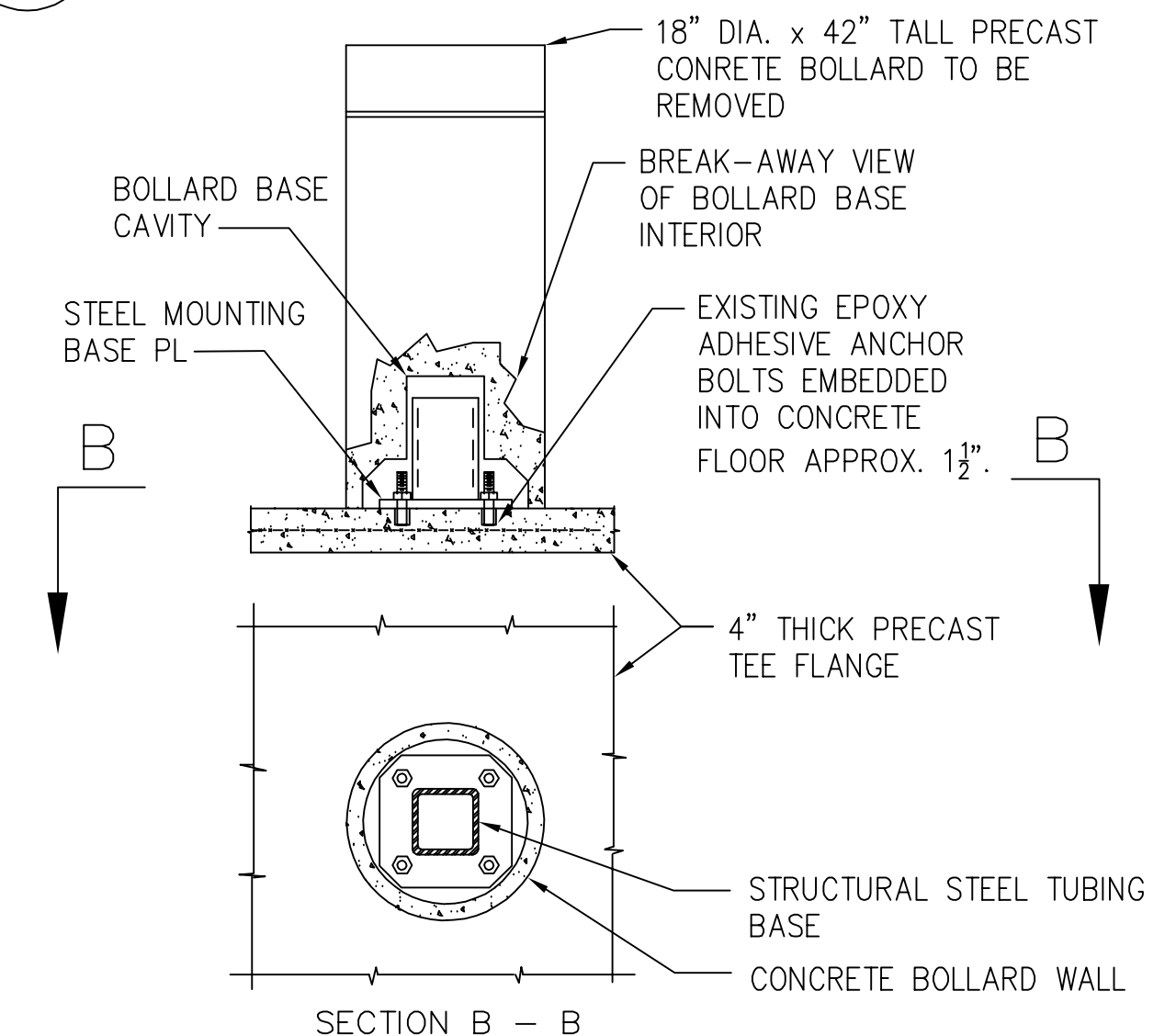


NOTES (ITEM 8.4):

1. PREPARE SURFACE AND INSTALL TRAFFIC DELINEATORS IN ACCORDANCE WITH THE EPOXY ADHESIVE MFR. INSTRUCTIONS.
2. TRAFFIC DELINEATOR INSTALLATIONS AT LOCATIONS WHERE NO TRAFFIC COATINGS ARE TO BE APPLIED (AT BARE CONCRETE) SHALL INCLUDE COVERING THE FLUSH-CUT ANCHOR BOLT STUBS WITH EPOXY ADHESIVE SUBSIDIARY TO THE WORK.

BOLLARDS & TRAFFIC CONTROL - INSTALL TRAFFIC DELINEATOR DEVICES

8.4 SCALE: NTS



NOTES (ITEM 8.1):

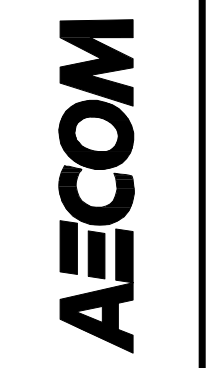
1. LIFT AND REMOVE CONCRETE BOLLARD FROM STEEL BASE POST, REMOVE ANCHOR BOLT NUTS AND STEEL BASE. USE CAUTION TO AVOID DAMAGE TO FLOOR.
2. CUT-OFF EXISTING ANCHOR BOLTS FLUSH TO FLOOR AND GRIND SMOOTH WITH SLIGHT RECESS INTO (BELOW) FLOOR TEXTURE RIDGES. REMOVE ALL BURRS AND SHARP EDGES.
3. ANCHOR BOLT HOLES FOR ANCHORS THAT PULL OUT OF THE FLOOR (ADHESIVE BONDING FAILURE) SHALL BE THOROUGHLY MECHANICALLY CLEANED AND FILLED WITH NON-SHRINK GROUT.
4. COAT OVER THE CUT-OFF BOLTS AND FILLED HOLES WHERE TRAFFIC TOPPING ARE DESIGNATED FOR INSTALLATION.
5. REPAIR ANY PREVIOUSLY DAMAGED (SPALLED) FLOOR AREAS UNDER WORK ITEM 2.3. ANY AREAS THAT ARE DAMAGED BY BOLLARD REMOVAL WORK SHALL BE REPAIRED IN ACCORDANCE WITH ITEM 2.3 AT NO EXPENSE TO THE OWNER.
6. ALL BOLLARDS SHALL BE REMOVED FROM THE GARAGE AND NEATLY PLACED AT THE MAINTENANCE DRIVEWAY AREA LOCATED AT THE NORTH END OF THE PARKING GARAGE AS DIRECTED BY THE OWNER. CONTRACTOR SHALL DISPOSE OF STEEL BASES SUBSIDIARY TO THIS WORK. DISPOSAL OF CONCRETE BOLLARDS IS UNDER WORK ITEM 8.2.

BOLLARDS & TRAFFIC CONTROL - CONCRETE BOLLARD REMOVAL

8.1 SCALE: NTS

NO.	DATE	MADE BY	CHECKED	DESCRIPTION

1155 ELM STREET, SUITE 401
MANCHESTER, NH 03101
(603) 686-4800



CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL-6 FLOOR & LEVEL-5 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY24-805-21

PROJECT NO:	60719585-T03
CAD DWG FILE:	8-10_RD.22-1-3
DESIGNED BY:	JGG
DRAWN BY:	CMP
DEPT CHECK:	MAB
PROJ CHECK:	RJD
DATE:	APRIL 2024
SCALE:	AS NOTED
RD.24-F3	
SHEET 10 OF 13	

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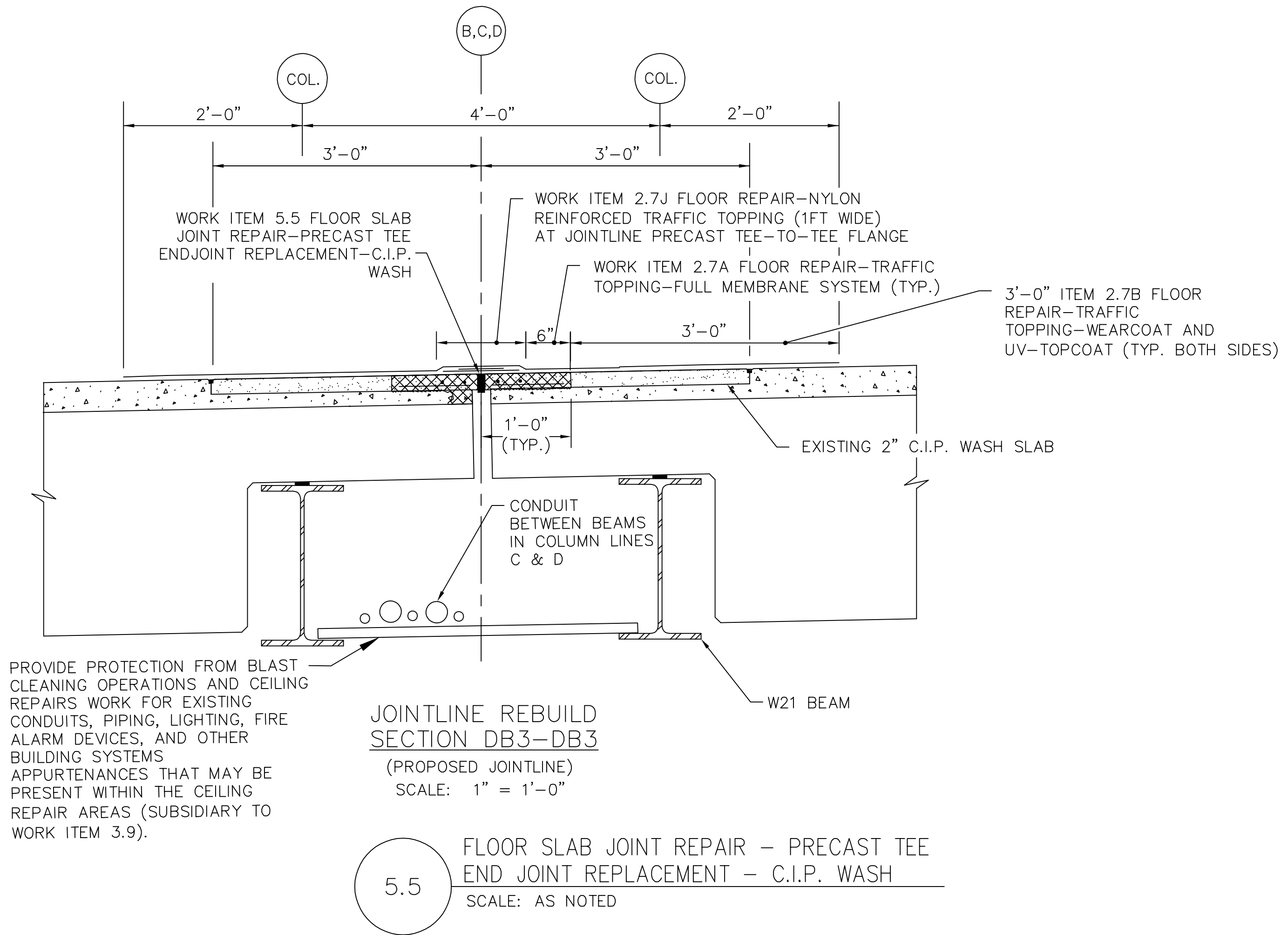
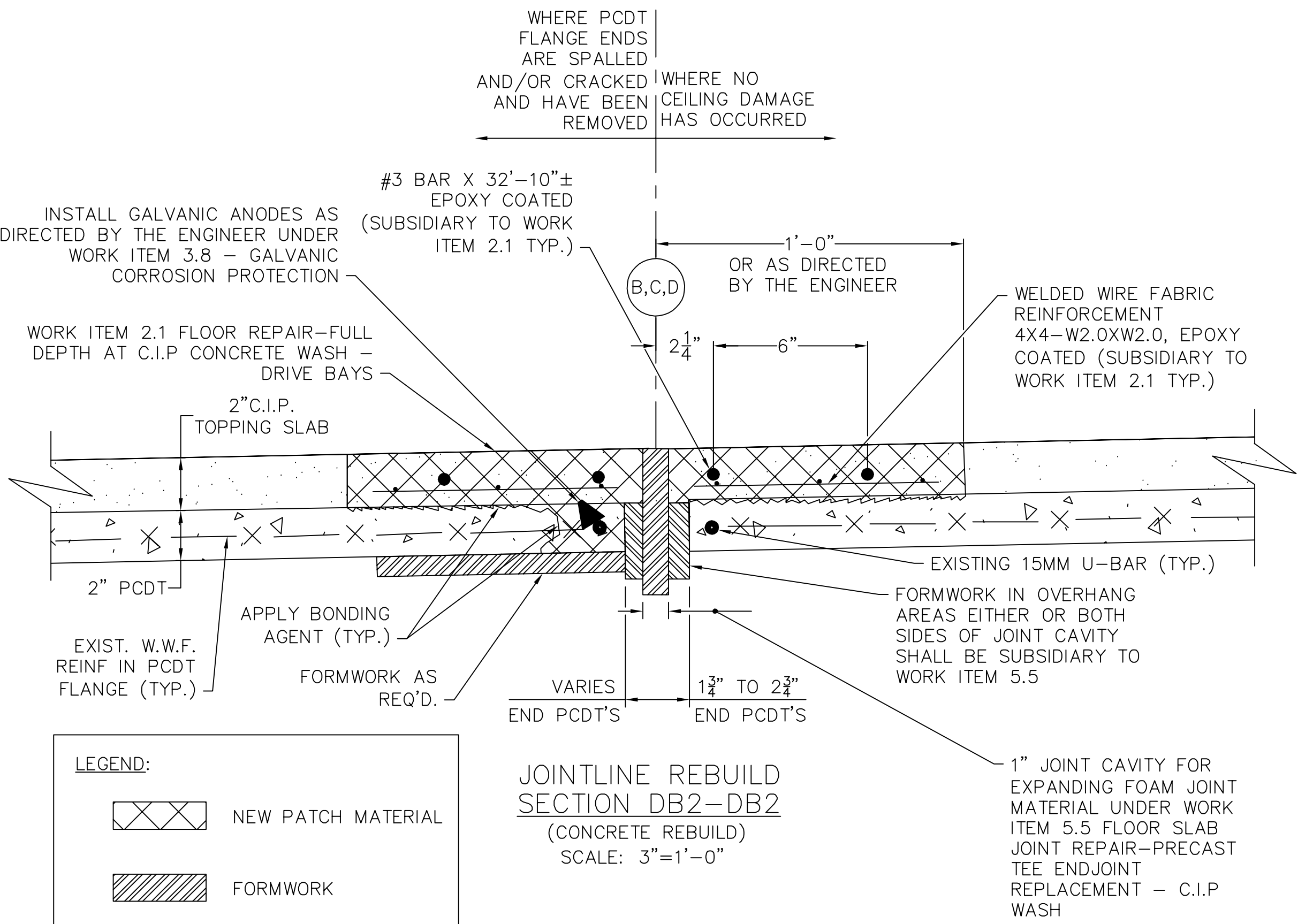
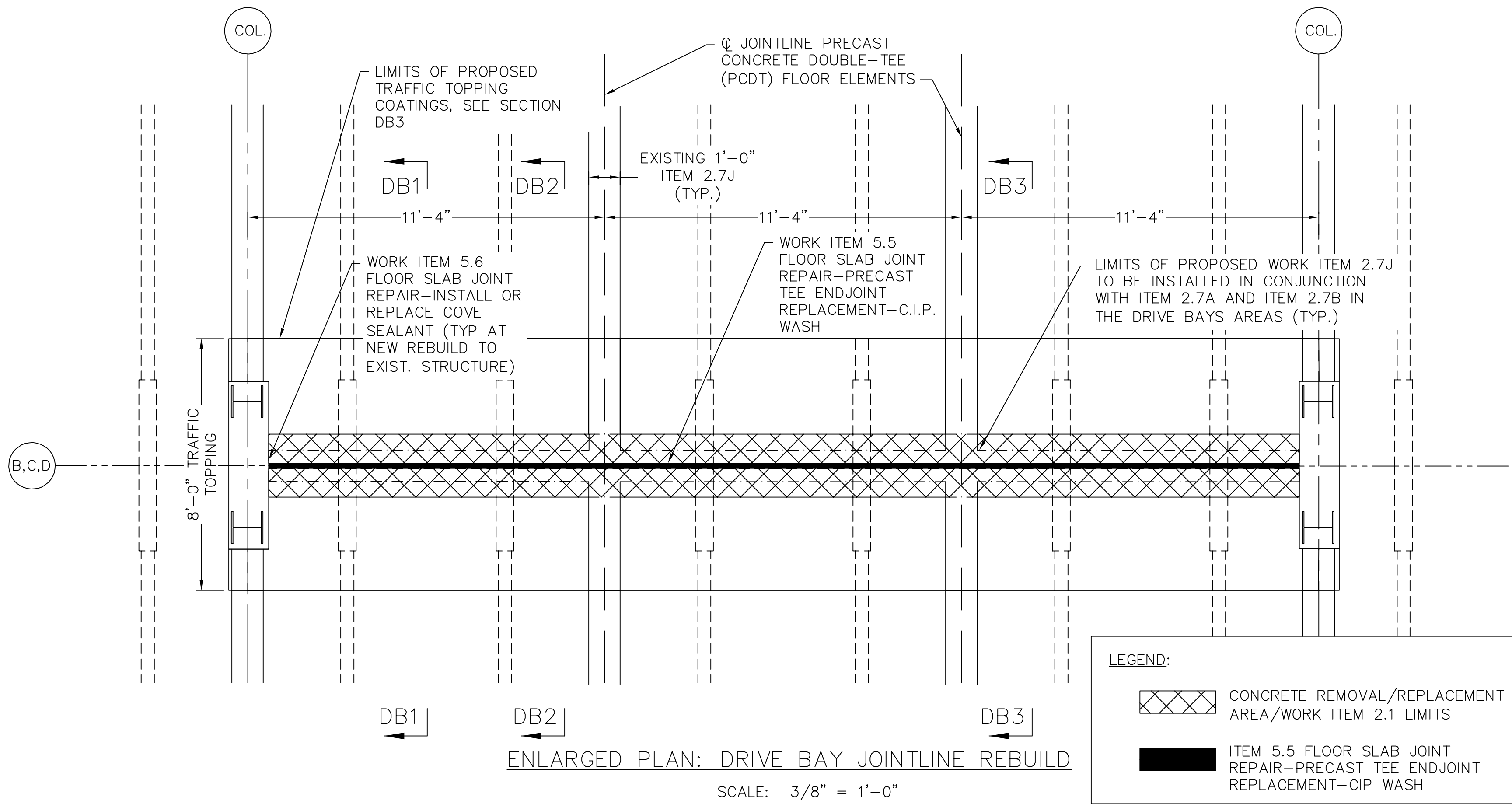
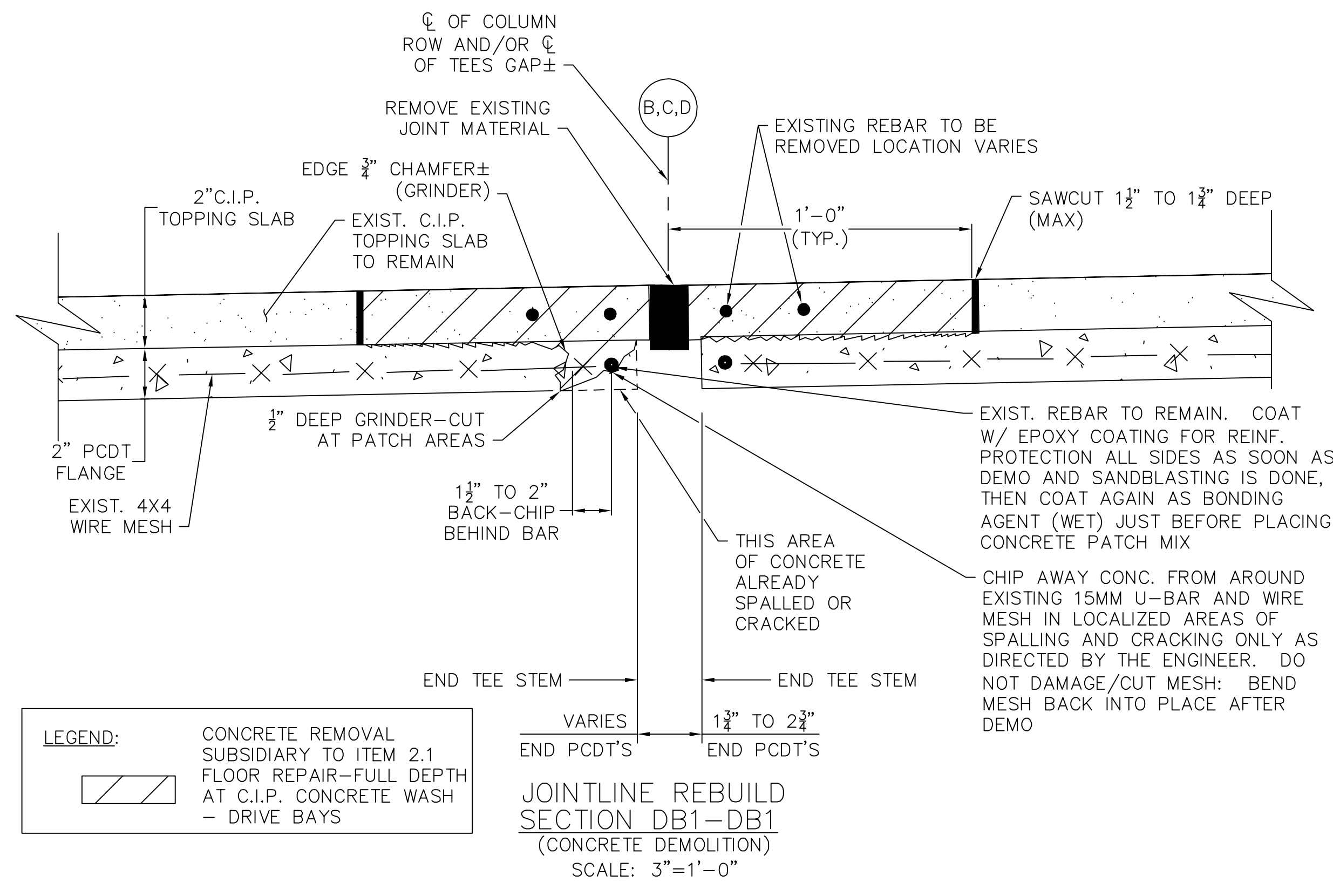
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2.1 FLOOR REPAIR - FULL DEPTH AT C.I.P. CONCRETE WASH - DRIVEBAYS
SCALE: AS NOTED

5.5 FLOOR SLAB JOINT REPAIR - PRECAST TEE END JOINT REPLACEMENT - C.I.P. WASH
SCALE: AS NOTED

1555 ELM STREET, SUITE 401
MANCHESTER, NH 03101
(603) 686-4800

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CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL-6 FLOOR & LEVEL-5 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY24-805-21

PROJECT NO:	60719585-T03
CAD DWG FILE:	11-13_RD.24
DESIGNED BY:	JGG
DRAWN BY:	CMP
DEPT CHECK:	MAB
PROJ CHECK:	RJD
DATE:	APRIL 2024
SCALE:	AS NOTED

RD.24-F4
SHEET 11 OF 13

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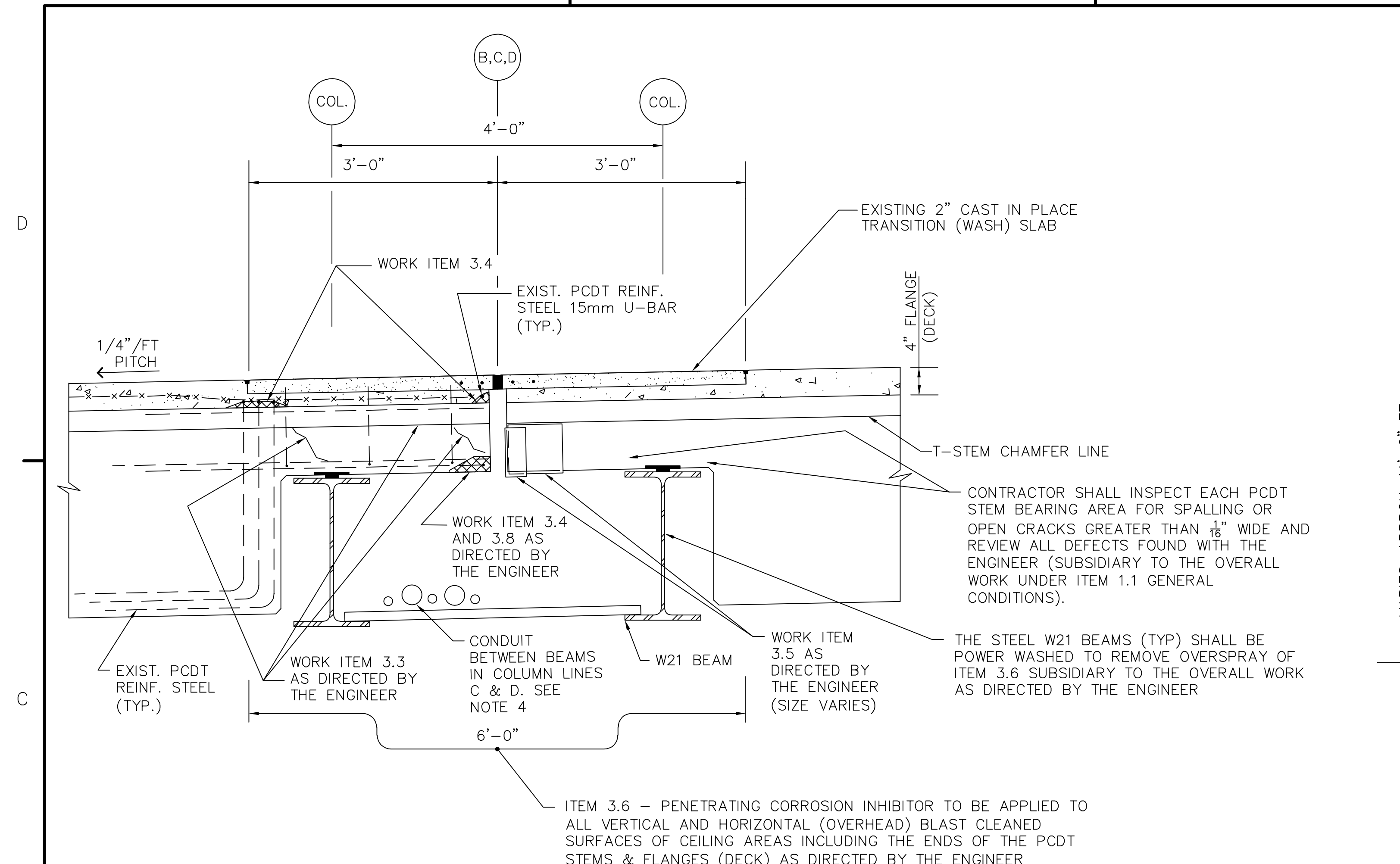
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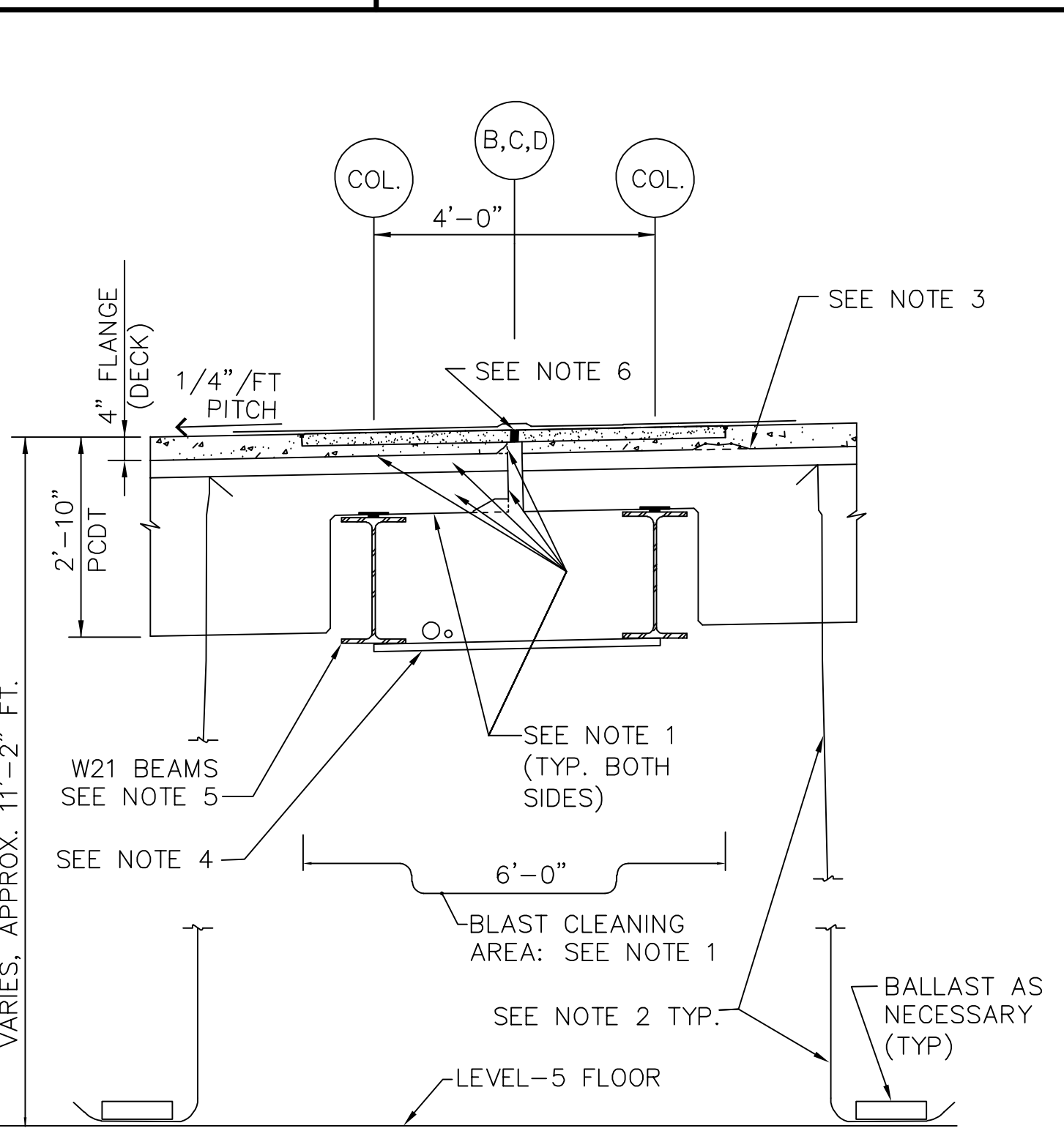
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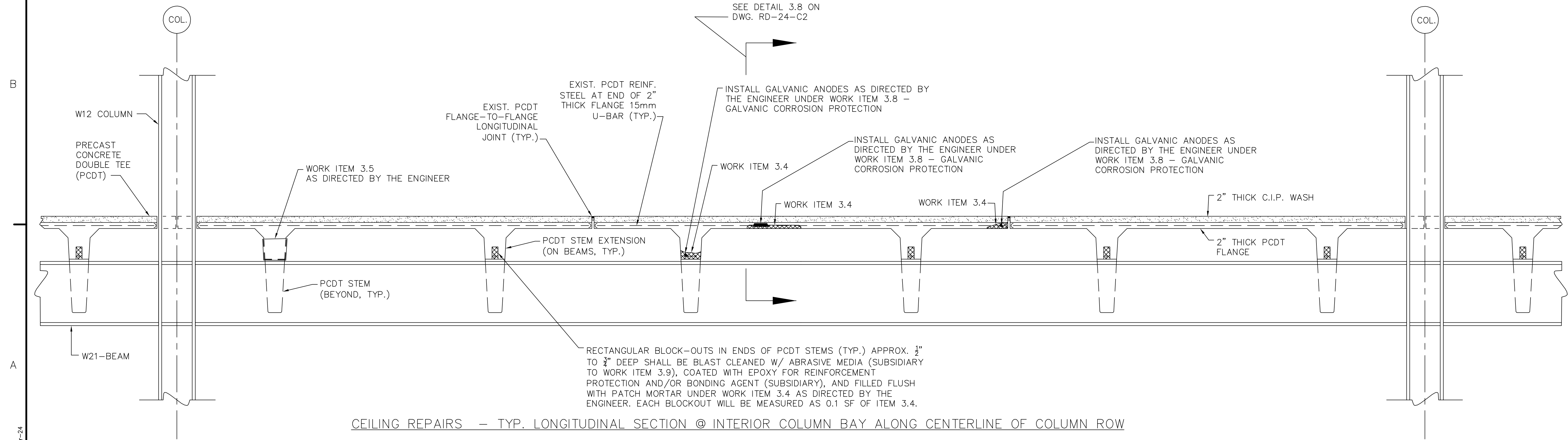
CEILING REPAIRS - TYP. SECTION @ INTERIOR COLUMN ROW (COLUMN LINE E SIMILAR)
(LOOKING NORTH)
SCALE: 1" = 1'-0"



CEILING REPAIRS - TYP. SECTION AT COL. ROWS -
DUST/DEBRIS SHIELDING & SURFACE
PREPARATION (COLUMN LINE E SIMILAR)
(LOOKING NORTH)
SCALE: 1/2" = 1'-0"

NOTES:

1. AFTER REMOVING (CHIPPING) ALL LOOSE, CRACKED, AND DETERIORATED CONCRETE AS DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL BLAST CLEAN ALL CONCRETE CEILING SURFACES, INCLUDING THE ENDS OF PCDT STEMS AND FLANGE (DECK) ENDS, AND EXPOSED REINFORCING STEEL AND STEEL EMBEDMENTS UNDER WORK ITEM 3.9 - BLAST CLEANING W/ ABRASIVE MEDIA. BLAST CLEANING SHALL PREPARE THE SURFACE OF ALL AREAS TO RECEIVE PATCHING MATERIALS AND APPLICATION OF PENETRATING CORROSION INHIBITOR. EXPOSED REINFORCING STEEL AND EMBEDMENTS SHALL BE PREPARED TO A NEAR-WHITE METAL SURFACE PREP (SSPC-SP10) AND SHALL RECEIVE ONE COAT OF EPOXY PROTECTIVE COATING (SUBSIDIARY TO ITEM 3.4) WITHIN 4 HOURS OF BLAST CLEANING.
2. CONTRACTOR-DESIGNED TEMPORARY D/D CONTAINMENT AND SHIELDING FOR BLAST CLEANING OPERATIONS SHALL BE INSTALLED TO PREVENT EXCESSIVE ERRANT MEDIA AND DUST FROM MIGRATING TO OTHER AREAS OF THE GARAGE. INSTALL CONTAINMENT AND/OR SHIELDING AS APPROVED BY THE ENGINEER FROM THE CEILING (UNDERSIDE OF DECK) TO THE FLOOR SURFACE AS REQUIRED TO CONTAIN DUST & DEBRIS AND PREVENT WIND-BLOWN DISPERSION OF DUST. DUST SHALL BE CLEANED UP DAILY AS SOON AS BLAST CLEANING OPERATIONS CEASE TO PREVENT WIND DISPERSION.
3. LOCALIZED AREAS OF CEILING DETERIORATION (SPALLING) OUTSIDE OF THE TYPICAL BLAST CLEANING AREA SHALL BE BLAST CLEANED SUBSIDIARY TO THE WORK (AND BASE QUANTITY).
4. PROVIDE PROTECTION FROM BLAST CLEANING OPERATIONS AND CEILING REPAIRS WORK FOR EXISTING CONDUITS, PIPING, LIGHTING, FIRE ALARM DEVICES, AND OTHER BUILDING SYSTEMS APPURTENANCES THAT MAY BE PRESENT WITHIN THE CEILING REPAIR AREAS (SUBSIDIARY TO WORK ITEM 3.9).
5. LOCALIZED D/D SHIELDING SHALL BE PROVIDED AS NECESSARY TO PROTECT THE EXISTING GALVANIZED BEAMS AND PAINTED COLUMNS FROM SURFACE COATING ABRASION. THE CONTRACTOR SHALL PROVIDE TOUCH-UP OF DAMAGE TO COATED SURFACES, AND REPAIR OF BUILDING SYSTEMS APPURTENANCES AS DIRECTED BY THE OWNER, AT NO COST TO THE OWNER.
6. USE CAUTION WHILE SANDBLASTING TO AVOID DAMAGE TO EXIST. JOINTLINE MATERIALS BETWEEN PCDT'S ABOVE THE WORK AREAS.
7. THE CONTRACTOR SHALL PROVIDE ALL SAFETY & ENVIRONMENTAL CONTROL MEASURES, MEDIA & DUST DISPOSAL, AND PERSONAL PROTECTIVE EQUIPMENT FOR BLAST CLEANING OPERATIONS IN ACCORDANCE WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS (SUBSIDIARY).

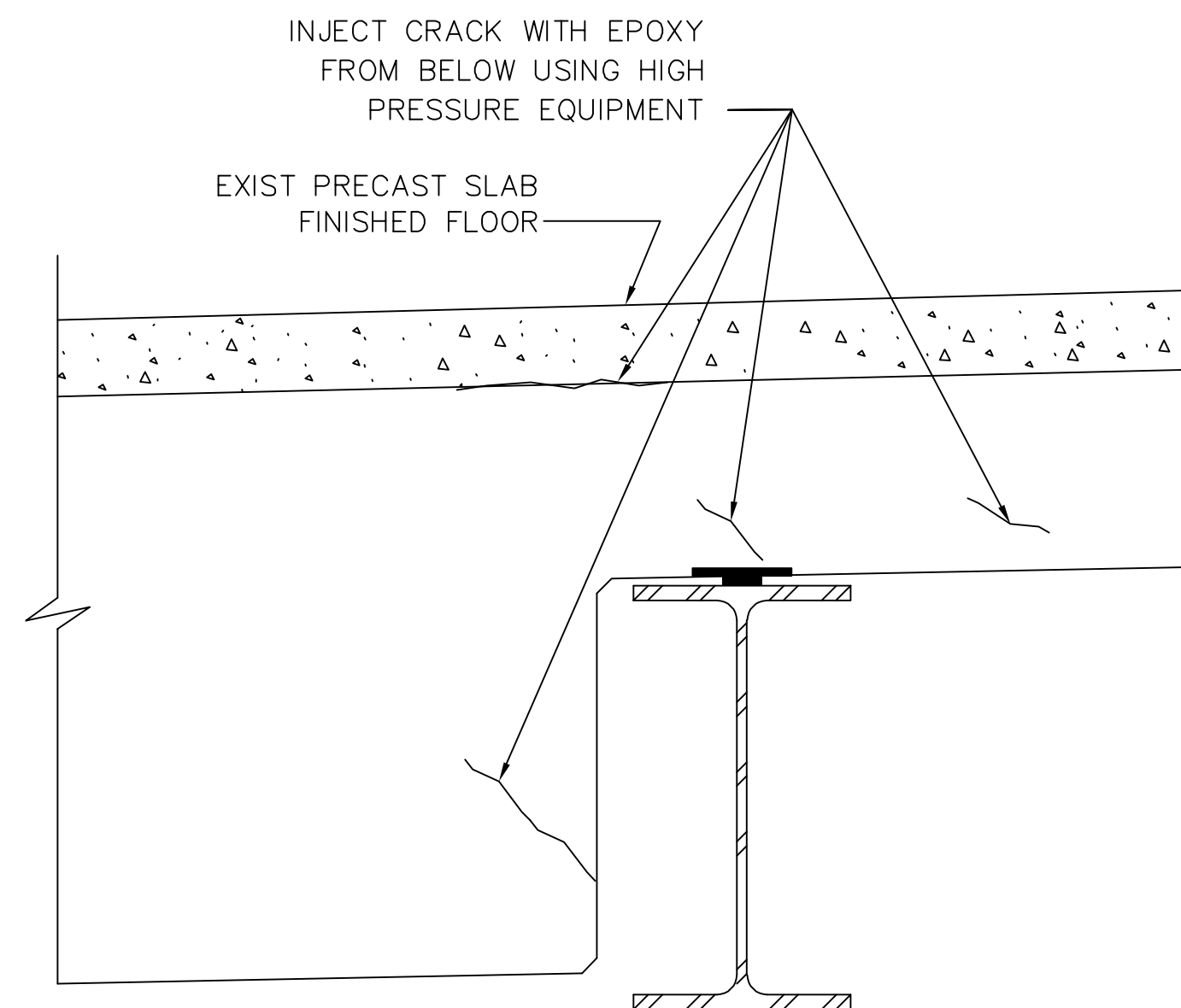


CEILING REPAIRS - TYP. LONGITUDINAL SECTION @ INTERIOR COLUMN BAY ALONG CENTERLINE OF COLUMN ROW
(LOOKING EAST OR WEST)
SCALE: 3/4" = 1'-0"

<p>PROJECT NO: 60719585-103 CAD DWG FILE: 11-13_RD.24 DESIGNED BY: JGG DRAWN BY: CMP DEPT CHECK: MAB PROJ CHECK: RJD DATE: APRIL 2024 SCALE: AS NOTED</p>											
<p>RD.24-C1 SHEET 12 OF 13</p>											
<p>CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION MANCHESTER - BOSTON REGIONAL AIRPORT PARKING GARAGE - LEVEL-6 FLOOR & LEVEL-5 CEILING SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS FY24-805-21</p>											
<p>AECOM 1155 ELM STREET, SUITE 401 MANCHESTER, NH (603) 686-4600</p>	<p>REVISIONS</p> <table border="1"> <thead> <tr> <th>MARK</th> <th>DATE</th> <th>MADE BY</th> <th>CHECKED</th> <th>DESCRIPTION</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	MARK	DATE	MADE BY	CHECKED	DESCRIPTION					
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ARCH D - 14-Apr-24

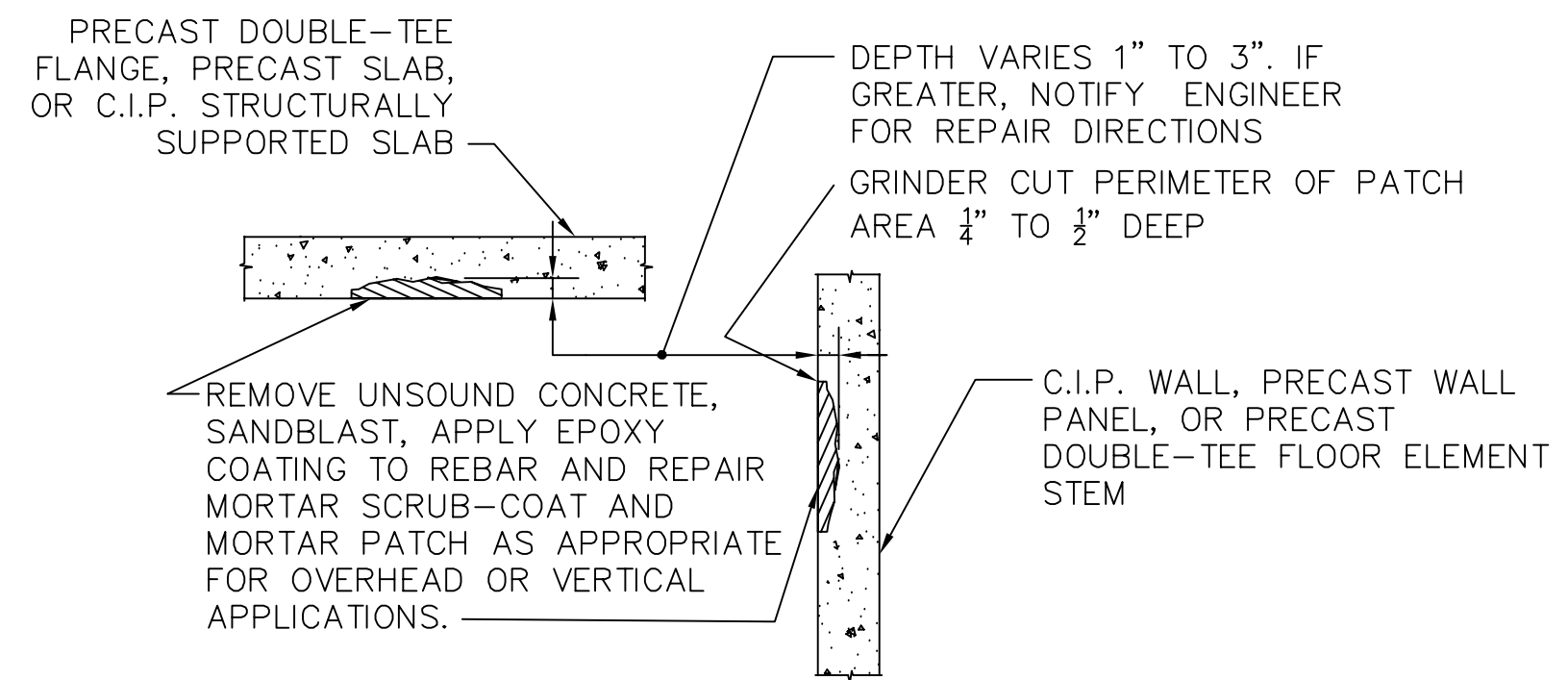
LEGEND	
	WORK ITEM NUMBER
	DETERIORATED CONCRETE REMOVAL
	NEW PATCH MATERIAL



NOTES (ITEM 3.3):

1. THE CONTRACTOR SHALL INSPECT ALL AREAS TO LOCATE CRACKS FOR POTENTIAL INJECTION AND PRE-MARK CRACK LOCATIONS ON A PLANS WORKSHEET FOR PRIOR REVIEW WITH AND APPROVAL BY THE ENGINEER. THE ENGINEER SHALL REVIEW AND MARK THE APPROVED CRACK LOCATIONS WITH THE CONTRACTOR'S SITE REPRESENTATIVE PRIOR TO COMMENCEMENT OF CRACK GRINDING PREPARATIONS.
2. ADDITIONAL CRACKS IN SOME AREAS MAY BE IDENTIFIED/NOTICED BETWEEN SUBSEQUENT PARTS OF THE REPAIR WORK AND ADDITIONAL CRACK INJECTION SHALL BE PERFORMED AS DIRECTED BY THE ENGINEER.
3. ONLY CRACK INJECTION LOCATIONS APPROVED, MARKED, AND RECORDED BY THE ENGINEER SHALL BE PAID.
4. GROOVE CRACKS WITH GRINDER AND CLEAN CRACKS OF LOOSE MATERIALS AND DEBRIS USING MECHANICAL MEANS AND COMPRESSED AIR BLAST.
5. INSTALL PORT ADAPTERS TO SURFACE IN ACCORDANCE WITH APPROVED EPOXY INJECTION RESIN MANUFACTURER'S INSTRUCTIONS. A MINIMUM OF TWO INJECTION PORTS ARE REQUIRED ON EACH CRACK.
6. SEAL SURFACE OF CRACK BY APPLYING EPOXY PASTE AND ALLOW IT TO HARDEN.
7. INJECT EPOXY USING CONSTANT PRESSURE INTO CRACK AT PORTING ADAPTERS. BEGIN INJECTION AT FIRST PORT AND CONTINUE UNTIL THERE IS APPEARANCE OF EPOXY AT THE NEXT PORT. WORK CRACKS FROM THE LOW ELEVATION TOWARDS HIGHER ELEVATION.
8. REMOVE ALL PORTING ADAPTERS AND REMOVE ALL SURFACE SEALER BY GRINDING FLUSH TO CONC. SURFACE.
9. REFER TO SPECIFICATIONS FOR ADDITIONAL INFO.

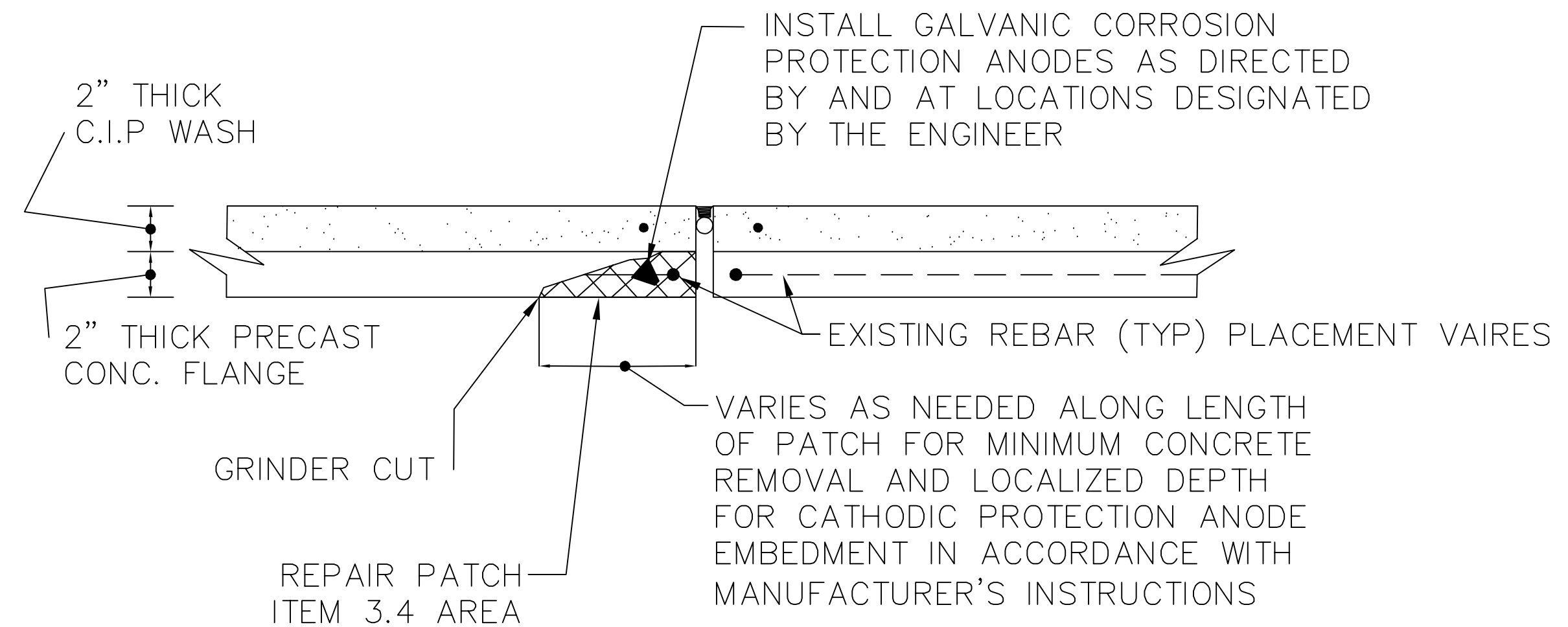
3.3 CEILING REPAIR – EPOXY CRACK INJECTION
SCALE: NTS



NOTES (ITEM 3.4):

1. THE CONTRACTOR SHALL HAMMER-SOUND AND INSPECT TO LOCATE AND MARK AREAS FOR REPAIRS. REVIEW MARKED AREAS WITH THE ENGINEER FOR APPROVAL PRIOR TO COMMENCEMENT OF EDGE CUTTING.
2. REMOVE DETERIORATED CONCRETE TO SOUND CONCRETE.
3. CLEAN ALL CONCRETE CAVITY SURFACES AND EXPOSED REINF. BARS BY BLAST CLEANING W/ ABRASIVE MEDIA (SUBSIDIARY); COAT ALL EXPOSED REINF. BARS WITH EPOXY PROTECTIVE COATING SUBSIDIARY TO THIS WORK ITEM (MASTER EMACO P 124 , SIKA ARMATEC 110 EPOCEM, OR APPROVED EQUAL.).
4. APPLY REPAIR MORTAR SCRUB-COAT AND MORTAR ACCORDING TO MANUFACTURER'S INSTRUCTIONS AND FINISH TO MATCH ADJOINING SURFACES.
5. FOR EXPOSED ARCHITECTURAL PRECAST PANEL REPAIRS (IF REQUIRED), (AT ELEVATOR TOWER, ETC.) EFFORTS SHALL BE MADE TO MATCH THE COLOR AND TEXTURE OF THE EXISTING WALL PANELS AS CLOSELY AS POSSIBLE. INSTALLER TO FIELD PREPARE TEST SAMPLE OF PATCHING MATERIAL FOR APPROVAL BY THE OWNER PRIOR TO THE APPLICATION OF THE REPAIR.

3.4 CEILING REPAIR – OVERHEAD CONCRETE MORTAR REPAIR
SCALE: N.T.S.



3.8 CEILING REPAIR – GALVANIC CORROSION PROTECTION
SCALE: NTS

AECOM
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CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL-6 FLOOR & LEVEL-5 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
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RD.24-C2
SHEET 13 OF 13

ARCH D - 14-Apr-24