

MANCHESTER•BOSTON REGIONAL AIRPORT

PARKING GARAGE: LEVEL-4 FLOOR & LEVEL-3 CEILING SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS (FY22-805-50)



**MANCHESTER•BOSTON
REGIONAL AIRPORT**

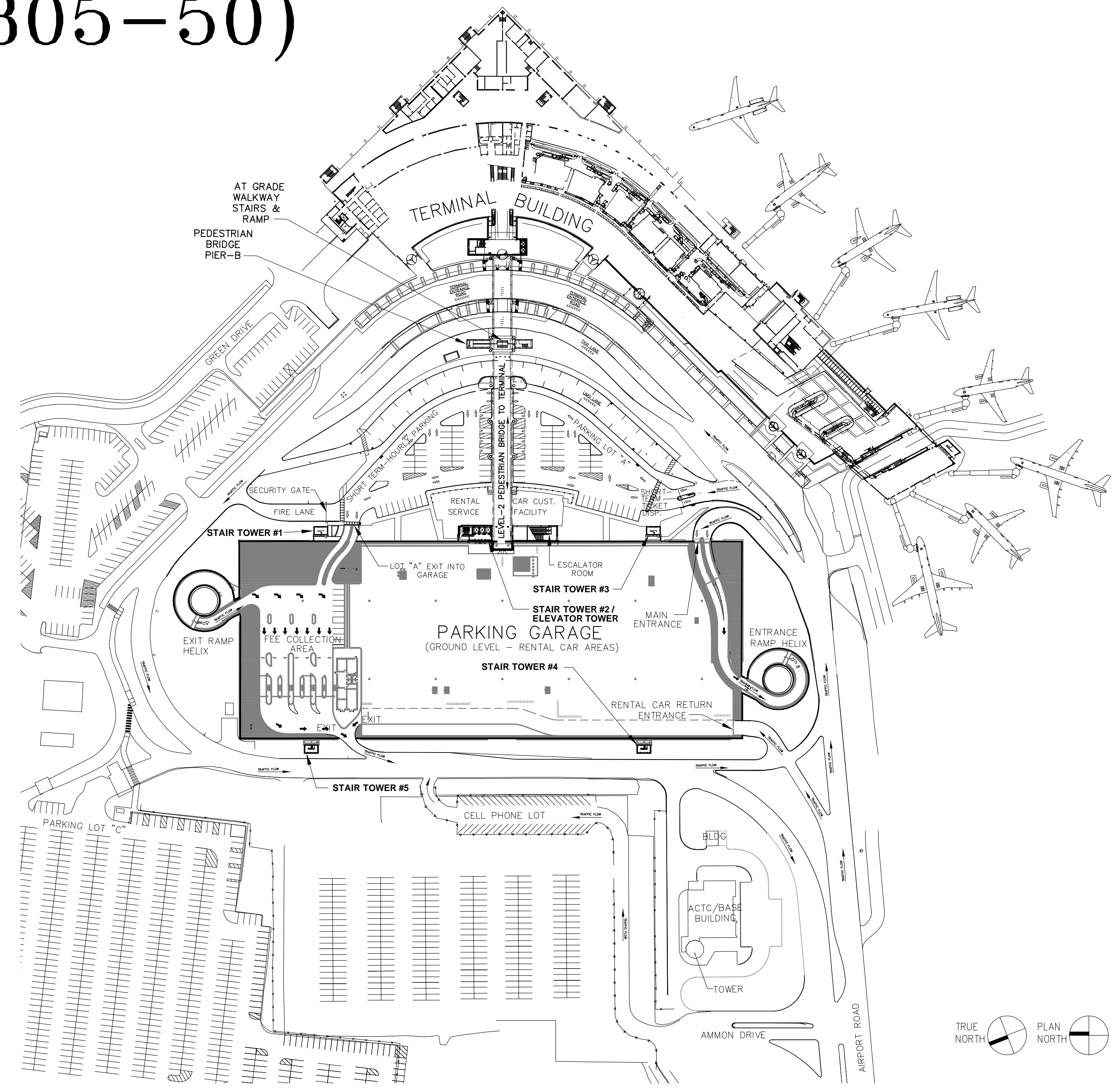
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MANCHESTER, NH 03101
PROJECT # 60638480_T18

LIST OF DRAWINGS

SHEET #	DRAWING NO.	TITLE
GENERAL:		
1	TS.22-1	TITLE SHEET & AIRPORT SITE PLAN
2	GN.22-1	GENERAL NOTES & WORK ITEM LIST
3	PZ.22-3/4	PHASING AND TRAFFIC PLAN: LEVEL 3 & 4
REPAIR PLANS:		
4	RP.22-FLR.4N	REPAIR PLAN: LEVEL 4 – FLOOR / NORTH END
5	RP.22-FLR.4S	REPAIR PLAN: LEVEL 4 – FLOOR / SOUTH END
6	RP.22-CLG.3N	DRIVE BAY & CEILING REPAIR PLAN: LEVEL-3 CEILING (NORTH)
7	RP.22-CLG.3S	DRIVE BAY & CEILING REPAIR PLAN: LEVEL-3 CEILING (SOUTH)
8	RP.22-PBST.1	REPAIR PLAN: LEVEL-1 PEDESTRIAN BRIDGE PIER-B GRADE STAIRS & RAMP REPAIRS
REPAIR DETAILS:		
9	RD.22-F1	FLOOR REPAIR DETAILS – I
10	RD.22-F2	FLOOR REPAIR DETAILS – II
11	RD.22-F3	FLOOR REPAIR DETAILS – III
12	RD.22-F4	FLOOR REPAIR DETAILS – IV DRIVE BAY JOINTLINE REBUILD
13	RD.22-C1	CEILING REPAIR DETAILS – I
14	RD.22-C2	CEILING REPAIR DETAILS – II



AIRPORT SITE PLAN – GARAGE AREA
SCALE: 1:100

DATE: APRIL 2022
PROJECT NO. FY22-805-50
DWG. NO. TS.22-1

SHEET # 1 OF 14

PATH/FILENAME: \\S:\PROJECTS\US\BUS\PROJECTS\PROJECT-BOSTON\MANCHESTER AIRPORT - MNT\3_60638480.DWG\PLANNING SERVICES 2020-2023\TASK ORDER 18 - GARAGE 2022 -LAF\B&L\3\1500 CAD-05-Graphics\510 CAD\3_GN22-1_GENNOTES.DWG
LAST UPDATE: Monday, April 11, 2022 2:45:56 PM
PLOT DATE: Monday, April 11, 2022 2:50:10 PM
ARCH D - 14-Apr-22

D

C

B

A

ABBREVIATIONS:

ACI	AMERICAN CONCRETE INSTITUTE	JT.	JOINT
ADD'L	ADDITIONAL		
ADJ.	ADJACENT	K	KIP(S)
ALT.	ALTERNATE	K.S.I.	KIPS PER SQUARE INCH
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS	LB.(S)	POUND(S)
AWS & ®	AMERICAN WELDING SOCIETY AND AT	L.F.	LINEAR FEET
		LG.	LONG (LENGTH)
		L.P.	LOW POINT
BIT.	BITUMINOUS	MAX.	MAXIMUM
BM.	BEAM	MECH.	MECHANICAL
B.O.	BOTTOM OF	MEMB.	MEMBER (MEMBRANE)
BOT.	BOTTOM	MFG.	MANUFACTURER
BRG.	BEARING	MIN.	MINIMUM
C.F.	CUBIC FEET	NO.	NUMBER
C.I.P.	CAST-IN-PLACE	N.T.S.	NOT TO SCALE
CJ	CONSTRUCTION JOINT		
CMU	CONCRETE MASONRY UNIT	O.C.	ON-CENTER
COL.(S)	COLUMN(S)	OPNG.	OPENING
CONC.	CONCRETE	OPP.	OPPOSITE
CONN.	CONNECTION		
CONST.	CONSTRUCTION	P/C	PRECAST CONCRETE
CONT.	CONTINUOUS	P.C.I.	PRECAST CONCRETE INSTITUTE
CTR.	CENTER	PCDT	PRECAST CONCRETE DOUBLE TEE
		PEN.	PENETRATION
DIA.	DIAMETER	PL.	PLATE
DEMO.	DEMOLISH	POLY	POLYETHYLENE
DET.	DETAIL	PROJ.	PROJECTION
DIAG.	DIAGONAL	P.S.F.	POUNDS PER SQUARE FOOT
D.T.	DOUBLE TEE	P.S.I.	POUNDS PER SQUARE INCH
DWG.(S)	DRAWING(S)	PVC	POLYVINYL CHLORIDE
DWL.(S)	DOWEL(S)		
D/D	DUST/DEBRIS	REF.	REFERENCE
		REINF.	REINFORCEMENT (REINFORCED)
		REQ'D	REQUIRED
EA.	EACH	SCHED.	SCHEDULE
E.F.	EACH FACE	SECT.	SECTION
E.J. (E/J)	EXPANSION JOINT	S.F.	SQUARE FEET
EL.	ELEVATION	SIM.	SIMILAR
ELEC.	ELECTRICAL	SP.	SPACES
EMBED.	EMBEDMENT	SQ.	SQUARE
EQ.	EQUAL	SST	STAINLESS STEEL
E.W.	EACH WAY	STD.	STANDARD
EWEF	EACH WAY, EACH FACE	STOR.	STORAGE
EXIST.	EXISTING		
EXP.	EXPANSION	T&B	TOP AND BOTTOM
EXT.	EXTERIOR	T.A.R.	TYPICAL AS REQUIRED
		TB	TIE BEAM
FIN.	FINISHED	TEMP.	TEMPORARY (TEMPERATURE)
FL.	FLOOR	THK.	THICK
FND.	FOUNDATION	TJ	TOOLED JOINT
FT.	FOOT (FEET)	T.O.	TOP OF
FTG.	FOOTING	T.O.C.	TOP OF CONCRETE
		T.O.S.	TOP OF STEEL
GA.	GAGE	T.O.W.	TOP OF WALL
GAL.	GALLON(S)	TYP.	TYPICAL
GALV.	GALVANIZED	U.N.O.	UNLESS NOTED OTHERWISE
G.C.	GENERAL CONTRACTOR		
GR.	GRADE	V (VERT.)	VERTICAL
H.A.S.	HEADED ANCHOR STUD	V.E.F.	VERTICAL EACH FACE
HDP	HOT DIP GALVANIZED		
H.E.F.	HIGH-DENSITY POLYETHYLENE	W	WIDE OR WIDTH
HORIZ.	HORIZONTAL EACH FACE	W/C	WATER TO CEMENT RATIO
H.P.	HORIZONTAL	W.W.F.	WELDED WIRE FABRIC
HSS	HIGH POINT	W/	WITH
	HOLLOW STRUCTURAL SECTIONS		
IN.	INCH(ES)		
INFO.	INFORMATION		
INV.	INVERTED		
I.T.	INVERTED TEE		

SYMBOLS

=	EQUALS
>	GREATER THAN
<	LESS THAN
≥	GREATER THAN OR EQUAL
≤	LESS THAN OR EQUAL
%	PERCENT
±	PLUS/MINUS
#	NUMBER
f'c	SPECIFIED 28-DAY DESIGN COMPRESSIVE STRENGTH OF CONCRETE
Fy	SPECIFIED YIELD STRENGTH
∅	DIAMETER
#4@18	REINFORCING BAR SIZE AND SPACING

LEGEND:

1.1 BUBBLE NUMBER REFERS TO SPECIFIC REPAIR DETAIL IN THE DETAIL SHEETS AND/OR REFERS TO SPECIFICATIONS SECTION 02000 "WORK ITEMS".

GENERAL NOTES – PARKING GARAGE REPAIRS:

*CONSTRUCTION DOCUMENTS INCLUDE THE PLANS AND DETAILS ON THE FOLLOWING DRAWINGS AND SEPARATELY BOUND SPECIFICATIONS.

1. CONSTRUCTION:

- CONSTRUCTION SHALL BE IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE OF NEW HAMPSHIRE AND CITY OF MANCHESTER CODES AND ORDINANCES, INCLUDING FIRE CODES.
- ALL DRAIN AND PIPING WORK SHALL CONFORM TO LOCAL BUILDING CODES.
- ALL MATERIAL PROPERTIES SHALL BE AS NOTED IN THE SPECIFICATION.
- COORDINATE ALL UTILITIES SHUTDOWNS WITH OWNER PRIOR TO INTERRUPTING SERVICES.
- CONTRACTOR SHALL CONFORM TO TRAFFIC FLOW COORDINATION AND AREA CLOSURE REQUIREMENTS CONTAINED IN THE CONTRACT DOCUMENTS.
- CONTRACTOR SHALL LIMIT FLOOR LOADING WITHIN THE WORK AREA TO NO MORE THAN 50 PSF. VEHICLE WHEEL LOADS SHALL BE LIMITED TO A MAXIMUM OF 2,000 LBS OVER A 20 SQ. IN. TIRE CONTACT AREA (FLOOR DESIGN LOAD). CONTRACTOR SHALL NOT STORE SUPPLIES OR PARK VEHICLES OVER ANY SHORED AREAS.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN THE FIELD.

2. INTENT OF THE REPAIR DRAWINGS:

- THE INTENT OF THE STRUCTURAL REPAIR DRAWINGS IS TO SHOW THE EXISTING STRUCTURAL FEATURES AND THE GENERAL LOCATIONS OF REPAIR WORK.
- BUBBLE NUMBER REFERS TO SPECIFIC REPAIR DETAIL IN THE DETAIL SHEETS AND/OR WORK ITEM IDENTIFIED IN SECTION 02000 OF THE PROJECT SPECIFICATIONS "WORK ITEMS". NOT ALL ITEMS ARE REPRESENTED WITH A DRAWING DETAIL.
- WHERE WORK ITEM BUBBLE IS NOTED TYP., IT MEANS THE WORK IS TYPICAL THROUGHOUT THE REFERENCED WORK AREA.
- WHERE WORK ITEM BUBBLE IS NOTED WITH T.A.R., THE WORK IS TYPICAL AS REQUIRED AND OCCURS AT AREAS IN ADDITION TO THE DESIGNATED LOCATION.
- CONTRACTOR SHALL ESTABLISH EXTENT AND LOCATION OF WORK AND REPAIR AREA AND BE VERIFIED BY THE ENGINEER PRIOR TO COMMENCEMENT OF WORK AT EACH AREA.
- REFER TO THE UNIT PRICE BID FORM FOR APPROXIMATED QUANTITIES FOR EACH WORK ITEM, AS APPLICABLE. QUANTITIES MAY VARY AND MAY BE INCREASED, DECREASED OR DELETED AT THE DISCRETION OF THE ENGINEER.

3. CONCRETE:

- CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI (28 DAY) CONFORMING TO NHDOT CLASS AA, AND SHALL BE HIGH EARLY STRENGTH MIX FOR A MINIMUM OF 3,000 PSI IN 5 DAYS. BAGGED MATERIAL MAY BE USED SUBJECT TO PRODUCT DATA, SUBMITTAL TO, AND APPROVAL BY, THE ENGINEER.
- EXPOSED CONCRETE EDGES SHALL HAVE A 1/2" CHAMFER.
- CONCRETE EXPOSED TO VIEW SHALL HAVE SMOOTH FORM FINISH WITH FINIS AND FORM MARKS REMOVED TO A SMOOTH, STONE RUBBED SURFACE.
- ALL REINF. SHALL MEET ASTM A615 REQUIREMENTS.
- REINFORCEMENT INTENDED TO BE WELDED SHALL MEET ASTM A706 (REQUIREMENTS FOR WELDABLE REINFORCEMENT).
- AT EXPOSED REINFORCEMENT WHERE CROSS-SECTIONAL LOSSES DUE TO DETERIORATION EXCEED 20 PERCENT, SPLICE WITH REINF. MATCHING EXISTING REINF. BAR SIZE. EXTEND LAP SPLICE REINFORCEMENT BEYOND DETERIORATED SECTION TO DEVELOP PROPER TENSION SPLICE OR, AT ENGINEERS DISCRETION, CONTRACTOR MAY BE DIRECTED TO USE MECHANICAL COUPLERS IN LIEU OF LAP SPLICE.
- ALL EXPOSED REINF. SHALL BE CLEANED BY SAND BLASTING, WIRE SCABBLING TO REMOVE ALL TRACES OF RUST DOWN TO WHITE METAL.
- COAT ALL EXPOSED REINF. WITH APPROVED EPOXY COATING.

4. REMOVAL OF MATERIAL:

- ALL MATERIAL REMOVED SHALL BE DONE WITH EXTREME CAUTION.
- FOR REMOVAL OF MATERIAL AT AREAS TO BE PATCHED, PROVIDE NEAT SAWCUT TO 1" MAXIMUM DEPTH AT EDGES TO REMAIN.
- NOTIFY ENGINEER IMMEDIATELY OF ANY FIELD CONDITIONS WHICH APPEAR TO DIFFER FROM THOSE SHOWN OR IMPLIED ON THE DRAWINGS.
- RESTORE PRECAST PIECES REQUIRING STRUCTURAL REPAIR.
- PROPERLY DISPOSE OF ALL MATERIALS REMOVED FROM THIS EXISTING STRUCTURE RELATED TO WORK COVERED BY THE CONTRACT DOCUMENTS.

5. PAY ITEMS:

- QUANTITIES FOR PAY ITEMS MUST BE VERIFIED BY THE FIELD ENGINEER AND/OR AUTHORIZED REPRESENTATIVE AND CONFIRMED BY THE GENERAL CONTRACTOR PRIOR TO SUBMISSION OF PAYMENT APPLICATIONS.

PHASING AND WORK ZONE CAPTURE NOTES:

- THE PROJECT SHALL BE PERFORMED IN (2) PHASES AS SHOWN ON THE PLANS AND DESCRIBED AS FOLLOWS:
PHASE 4S-F / 3S-C (LEVEL 4 SOUTH FLOOR & LEVEL 3 SOUTH CEILING): WORK AREA GENERALLY EXTENDS FROM COLUMN LINE 14.0 SOUTHWARD TO COLUMN LINE 26.
PHASE 4N-F / 3N-C (LEVEL 4 NORTH FLOOR & LEVEL 3 NORTH CEILING): WORK AREA GENERALLY EXTENDS FROM COLUMN LINE 14.0 NORTHWARD TO COLUMN LINE 2.
- THE OWNER (AIRPORT) WILL CLOSE AND CAPTURE THE WORK AREAS ON LEVEL-4 AND LEVEL-3 IN ADVANCE OF THE CONTRACTOR'S WORK IN ACCORDANCE WITH THE PROJECT MILESTONE SCHEDULE.
- THE OWNER WILL PLACE BARRICADES AND SIGNAGE AT THE HELIX ENTRANCES AND PEDESTRIAN ENTRANCES TO THE WORK AREAS. THE CONTRACTOR SHALL GIVE THE OWNER AT LEAST 2 WEEKS ADVANCE WRITTEN NOTICE (WITH EXPLANATION) IF THE MILESTONE SCHEDULE START-DATE OF A PHASE CANNOT BE MET.
- THE OWNER WILL FURNISH AND INSTALL VEHICULAR ACCESS LANE AND PEDESTRIAN WALKWAY TRAFFIC CONTROL DEVICES (BARRELS, ROPING, ETC.) AS ILLUSTRATED ON THE PLANS TO FACILITATE THE PERFORMANCE OF THE WORK. THE CONTRACTOR SHALL COORDINATE HIS WORK WITH THE OWNER TO VERIFY AND MONITOR THE ACCURACY OF SAFETY AREA PLACEMENT AROUND AND UNDER HIS WORK TO ENSURE THE SAFETY OF THE GENERAL PUBLIC AND PROTECTION OF PARKED PATRON VEHICLES. CONTRACTOR SHALL MAINTAIN AND/OR ADJUST DEVICES TO SUIT THE WORK AREAS DELINEATION AND PUBLIC SAFETY.
- THE OWNER WILL FURNISH AND INSTALL DIRECTIONAL SIGNAGE AS INDICATED ON THE PLANS AND AS REQUIRED FOR PUBLIC USE OF THE FACILITY DURING CONSTRUCTION. THE CONTRACTOR SHALL FURNISH AND INSTALL CONSTRUCTION HAZARD WARNING SIGNAGE SUBSIDIARY TO ITEM 1.1 GENERAL REQUIREMENTS (SEE TECHNICAL SPECIFICATION SECTION 02000).
- PUBLIC PEDESTRIAN ACCESS TO THE ELEVATOR LOBBY SHALL BE MAINTAINED AT ALL TIMES. SUB-PHASING OF LOCALIZED WORK AREAS IN FRONT OF THE DOORS WILL BE REQUIRED TO BE PERFORMED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOT BEGIN THE WORK IN THE NORTH PHASE AREA UNTIL SPECIFICALLY AUTHORIZED TO DO SO BY THE OWNER. THE WORK IN THE SOUTH PHASE AREA MUST BE COMPLETE AND PREPARED FOR PUBLIC PARKING PRIOR TO RELOCATING THE TRAFFIC CONTROL AND BEGINNING GENERAL WORK IN THE NEXT PHASE AREA.
- ISOLATED (UNOCCUPIED) SUB-WORK AREAS (AND SAFETY AREAS) IN THE NEXT PHASE AREA MAY BE CAPTURED BY THE CONTRACTOR, SUBJECT TO THE APPROVAL OF THE OWNER, AND LOCALIZED PREPARATORY WORK INITIATED DURING THE PARKED VEHICLE ATTRITION PERIOD (APPROX. 2 WEEKS) JUST PRIOR TO THE GENERAL CHANGE FROM THE SOUTH PHASE TO THE NORTH PHASE.

WORK ITEM LIST FOR WORK ITEM DEFINITIONS, SEE PROJECT SPECIFICATIONS SECTION 02000		
WORK ITEM	DESCRIPTION	UNITS
1.0	GENERAL REQUIREMENTS 1.1 GENERAL REQUIREMENTS – MOBILIZATION AND SUPPLEMENTARY GENERAL CONDITIONS*	LS
2.0	FLOOR REPAIRS 2.1 FLOOR REPAIR – FULL DEPTH AT C.I.P WASH – DRIVE BAYS 2.2 FLOOR REPAIR – PARTIAL DEPTH AT C.I.P WASH 2.3 FLOOR REPAIR – PARTIAL DEPTH – AT PRECAST CONCRETE FLOOR SLAB 2.6 FLOOR REPAIR – TRAFFIC TOPPINGS – PEDESTRIAN GRADE* 2.7A FLOOR REPAIR – TRAFFIC TOPPING – FULL MEMBRANE SYSTEM* 2.7B FLOOR REPAIR – TRAFFIC TOPPING – WEARCOAT AND UV-TOPCOAT 2.7J FLOOR REPAIR – TRAFFIC TOPPING AT JOINTLINE PRECAST TEE-TO-TEE FLANGE EDGE JOINTS, NYLON REINFORCED 2.7H-R FLOOR REPAIR – TRAFFIC TOPPINGS MEMBRANE REPAIR AT ENTRANCE & EXIT HELIX 2.8. FLOOR REPAIR – PENETRATING CONCRETE SEALER WITH MIGRATING CORROSION INHIBITOR (MCI)*	SF SF SF SF SF SF SF SF
3.0	CEILING REPAIRS 3.3 CEILING REPAIR – EPOXY CRACK INJECTION 3.4 CEILING REPAIR – OVERHEAD CONCRETE MORTAR REPAIR 3.5 CEILING REPAIR – GFRP FABRIC WRAP* 3.6 CEILING REPAIR – PENETRATING CORROSION INHIBITOR* 3.8 CEILING REPAIR – GALVANIC CORROSION PROTECTION 3.9 CEILING REPAIR – BLAST CLEANING W/ ABRASIVE MEDIA*	LF SF SF SF EA SF
5.0	FLOOR SLAB JOINT REPAIRS 5.2A FLOOR SLAB JOINT REPAIR – EXPANSION JOINT NOSING – SURFACE REPAIR 5.3 FLOOR SLAB JOINT REPAIR – REMOVE AND REPLACE FAILED T/T FLANGE JOINT SEALANTS 5.5 FLOOR SLAB JOINT REPAIR-PRECAST TEE ENDJOINT REPLACEMENT-C.I.P. WASH 5.6 FLOOR SLAB JOINT REAIR – INSTALL OR REPLACE COVE SEALANT	LF LF LF LF
8.0	BOLLARDS & TRAFFIC CONTROL 8.1 BOLLARDS & TRAFFIC CONTROL – CONCRETE BOLLARD REMOVAL 8.2 BOLLARDS & TRAFFIC CONTROL – CONCRETE BOLLARD DISPOSAL* 8.3 BOLLARDS & TRAFFIC CONTROL – PROVIDE TRAFFIC DELINEATOR DEVICES (MATERIAL)* 8.4 BOLLARDS & TRAFFIC CONTROL – INSTALL TRAFFIC DELINEATOR DEVICES	EA LS EA EA
9.0	FIELD ITEMS & ALLOWANCES 9.1 FIELD ITEM – PEDESTRIAN BRIDGE PIER-B – GRADE STAIRS & RAMP REPAIR 9.5 FIELD ITEM ALLOWANCE – GENERAL*	LS AL

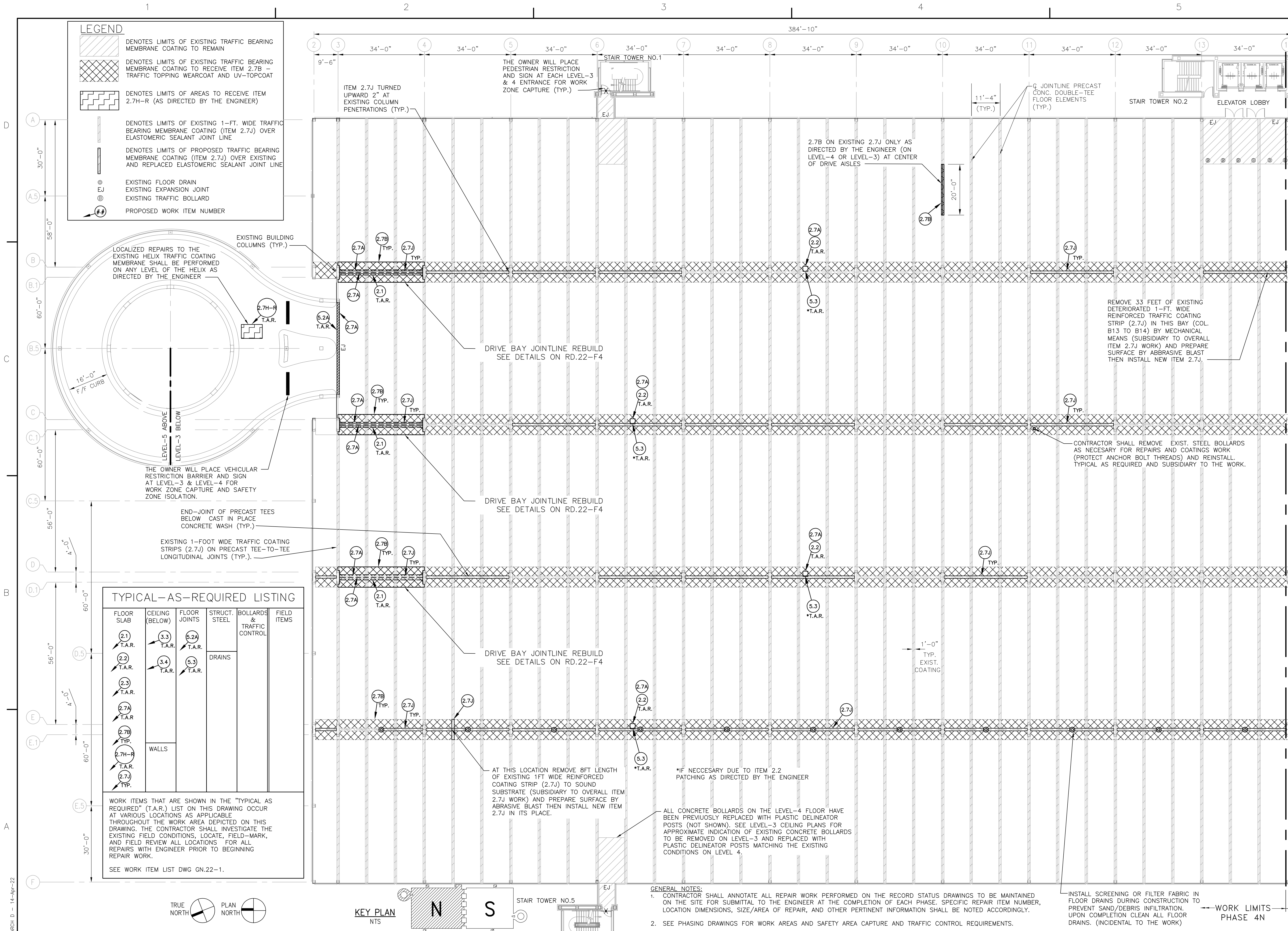
*NOT ALL WORK ITEMS ARE REPRESENTED BY A DRAWING DETAIL. REFER TO WORK ITEMS SCOPE AND TECHNICAL SPECIFICATIONS.

UNITS LEGEND:		LS	LF	EA
	LUMP SUM		LINEAR FEET	EACH
	SQUARE FOOT		T&M TIME & MATERIAL	AL ALLOWANCE
	MANDAY(8 HR)		N/A NOT APPLICABLE	

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CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL 4 FLOOR & LEVEL 3 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY22-805-50
GENERAL NOTES AND WORK ITEM LIST

PROJECT NO: 60638480-T18
CAD DWG FILE: 2_GN22-1
DESIGNED BY: JGG
DRAWN BY: CMP
DEPT CHECK: MAB
PROJ CHECK: RJG
DATE: APRIL 2022
SCALE: N/A
GN.22-1
SHEET 2 OF 14



LEGEND

DENOTES LIMITS OF EXISTING TRAFFIC BEARING MEMBRANE COATING TO REMAIN

DENOTES LIMITS OF EXISTING TRAFFIC BEARING MEMBRANE COATING TO RECEIVE ITEM 2.7B - TRAFFIC TOPPING WEARCOAT AND UV-TOPCOAT

DENOTES LIMITS OF AREAS TO RECEIVE ITEM 2.7H-R (AS DIRECTED BY THE ENGINEER)

DENOTES LIMITS OF EXISTING 1-FT. WIDE TRAFFIC BEARING MEMBRANE COATING (ITEM 2.7J) OVER ELASTOMERIC SEALANT JOINT LINE

DENOTES LIMITS OF PROPOSED TRAFFIC BEARING MEMBRANE COATING (ITEM 2.7J) OVER EXISTING AND REPLACED ELASTOMERIC SEALANT JOINT LINE

EXISTING FLOOR DRAIN

EXISTING EXPANSION JOINT

EXISTING TRAFFIC BOLLARD

PROPOSED WORK ITEM NUMBER

TYPICAL-AS-REQUIRED LISTING					
FLOOR SLAB	CEILING (BELOW)	FLOOR JOINTS	STRUCT. STEEL	BOLLARDS & TRAFFIC CONTROL	FIELD ITEMS
2.1 T.A.R.	3.3 T.A.R.	5.2A T.A.R.	DRAINS		
2.2 T.A.R.	3.4 T.A.R.	5.3 T.A.R.			
2.3 T.A.R.					
2.7A T.A.R.	WALLS				
2.7B TYP.					
2.7H-R T.A.R.					
2.7J TYP.					
WORK ITEMS THAT ARE SHOWN IN THE "TYPICAL AS REQUIRED" (T.A.R.) LIST ON THIS DRAWING OCCUR AT VARIOUS LOCATIONS AS APPLICABLE THROUGHOUT THE WORK AREA DEPICTED ON THIS DRAWING. THE CONTRACTOR SHALL INVESTIGATE THE EXISTING FIELD CONDITIONS, LOCATE, FIELD-MARK, AND FIELD REVIEW ALL LOCATIONS FOR ALL REPAIRS WITH ENGINEER PRIOR TO BEGINNING REPAIR WORK.					
SEE WORK ITEM LIST DWG GN.22-1.					

PROJECT NO: 60638480-T18

CAD DWG FILE: 4-5_RP.22-4

DESIGNED BY: JGG

DRAWN BY: CMP

DEPT CHECK: MAB

PROJ CHECK: RJD

DATE: APRIL 2022

SCALE: 1/16" = 1'-0"

CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION

MANCHESTER - BOSTON REGIONAL AIRPORT

PARKING GARAGE - LEVEL 4 FLOOR & LEVEL 3 CEILING

SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS

FY22-805-50

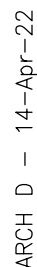
REPAIR PLAN: LEVEL - 4 FLOOR / NORTH END

RP.22-FLR.4N

SHEET 4 OF 14

REVISIONS

MARK	DATE	MADE BY	CHECKED	DESCRIPTION

















2. EXISTING PLYWOOD SHIELDING ON/BETWEEN STEEL BEAMS BELOW CEILING WORK AREAS TO BE REMOVED & SALVAGED TO OWNER OR DISPOSED OF AS DIRECTED BY THE ENGINEER (SUBSIDIARY TO THE OVERALL WORK).

DRIVE BAY JOINTLINE REBUILD
SEE DETAILS ON RD.22-F4
(TYP.)

TRA

LEVEL 2 BELOW	LEVEL 4 ABOVE
---------------	---------------

RIVE BAY JOINTLINE REBUILD
SEE DETAILS ON RD.22-F4
(TYP.)

FLOOR SLAB	CEILING (BELOW)	FLOOR JOINTS	STRUCT. STEEL	BOLLARDS & TRAFFIC CONTROL	FIELD ITEMS
 2.1 T.A.R.	 3.3 T.A.R.	 5.2A T.A.R.	DRAINS	 8.1 T.A.R.	
 2.2 T.A.R.	 3.4 T.A.R.	 5.3 T.A.R.		 8.2 T.A.R.	
 2.3 T.A.R.				 8.4 T.A.R.	
 2.7A T.A.R.					
 2.7B TYP.					
 2.7H-R T.A.R.	WALLS				
 2.7J TYP.					

WORK ITEMS THAT ARE SHOWN IN THE "TYPICAL AS REQUIRED" (T.A.R.) LIST ON THIS DRAWING OCCUR AT VARIOUS LOCATIONS AS APPLICABLE THROUGHOUT THE WORK AREA DEPICTED ON THIS DRAWING. THE CONTRACTOR SHALL INVESTIGATE THE EXISTING FIELD CONDITIONS, LOCATE, FIELD-MARK, AND FIELD REVIEW ALL LOCATIONS FOR ALL REPAIRS WITH ENGINEER PRIOR TO BEGINNING REPAIR WORK.

SEE WORK ITEM LIST DWG GN.22-1.

EJ	EXISTING EXPANSION JOINT
⊙	EXISTING FLOOR DRAIN
Ⓟ	EXISTING CONC. TRAFFIC BOLLARD

A schematic diagram of a magnetic circuit. It consists of a rectangular core with a North (N) pole on the left and a South (S) pole on the right. The core is made of a material with high permeability, indicated by diagonal hatching. The air gap between the poles is labeled 'g'. The magnetic flux Φ is shown as a series of arrows pointing from the North pole to the South pole through the air gap.

TRUE NORTH  PLAN NORTH 

CEILING PLAN

1. CONTRACTOR SHALL ANNOTATE ALL REPAIR WORK PERFORMED ON THE RECORD STATUS DRAWINGS TO BE MAINTAINED ON THE SITE FOR SUBMITTAL TO THE ENGINEER AT THE COMPLETION OF EACH PHASE. SPECIFIC REPAIR ITEM NUMBER, LOCATION DIMENSIONS, SIZE/AREA OF REPAIR, AND OTHER PERTINENT INFORMATION SHALL BE NOTED ACCORDINGLY.

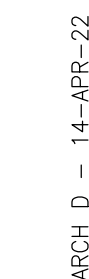
2. SEE PHASING DRAWINGS FOR WORK AREAS AND SAFETY AREA CAPTURE AND TRAFFIC CONTROL REQUIREMENTS.

AECOM

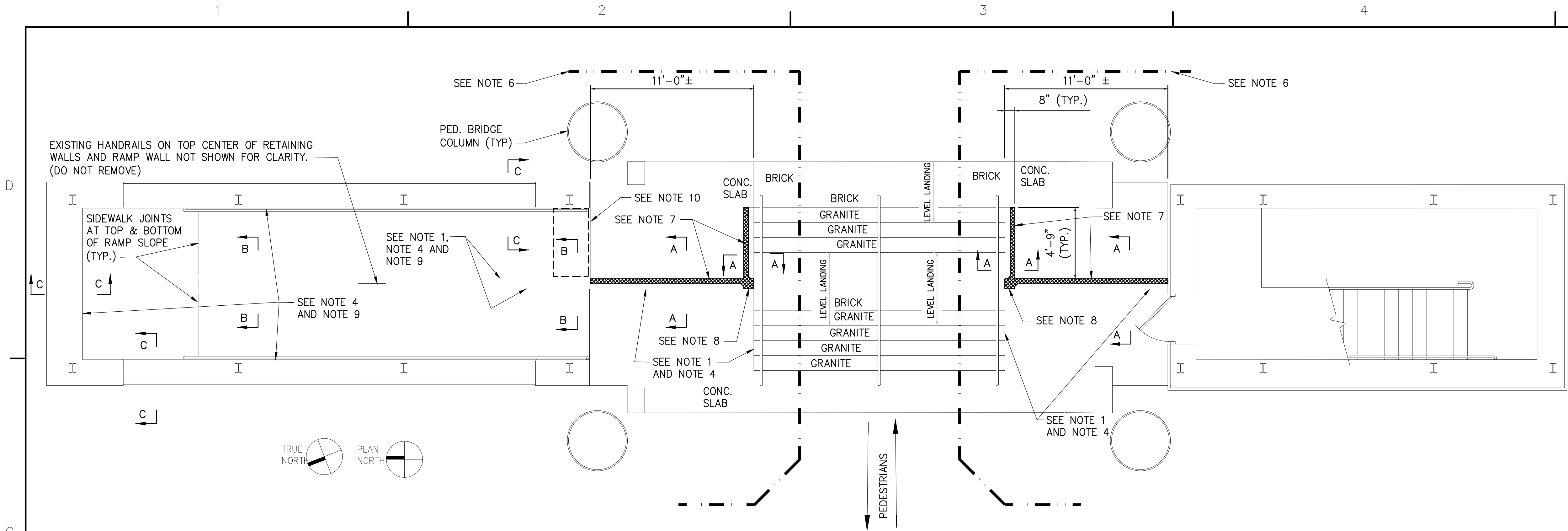
**CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER • BOSTON REGIONAL AIRPORT
PARKING GARAGE: LEVEL-4 FLOOR & LEVEL-3 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
F722-805-50
DRIVE BAY & CEILING REPAIR PLAN:
LEVEL-3 CEILING (NORTH)**

PROJECT NO:	60638480-T18
DWG FILE:	6-7_RP.22-3
DESIGNED BY:	JGG
DRAWN BY:	CMP
DEPT CHECK:	MAB
PROJ CHECK:	RJD
DATE:	APRIL 2022
SCALE:	AS NOTED

RP.22-CLG.3N
SHEET 6 OF 14

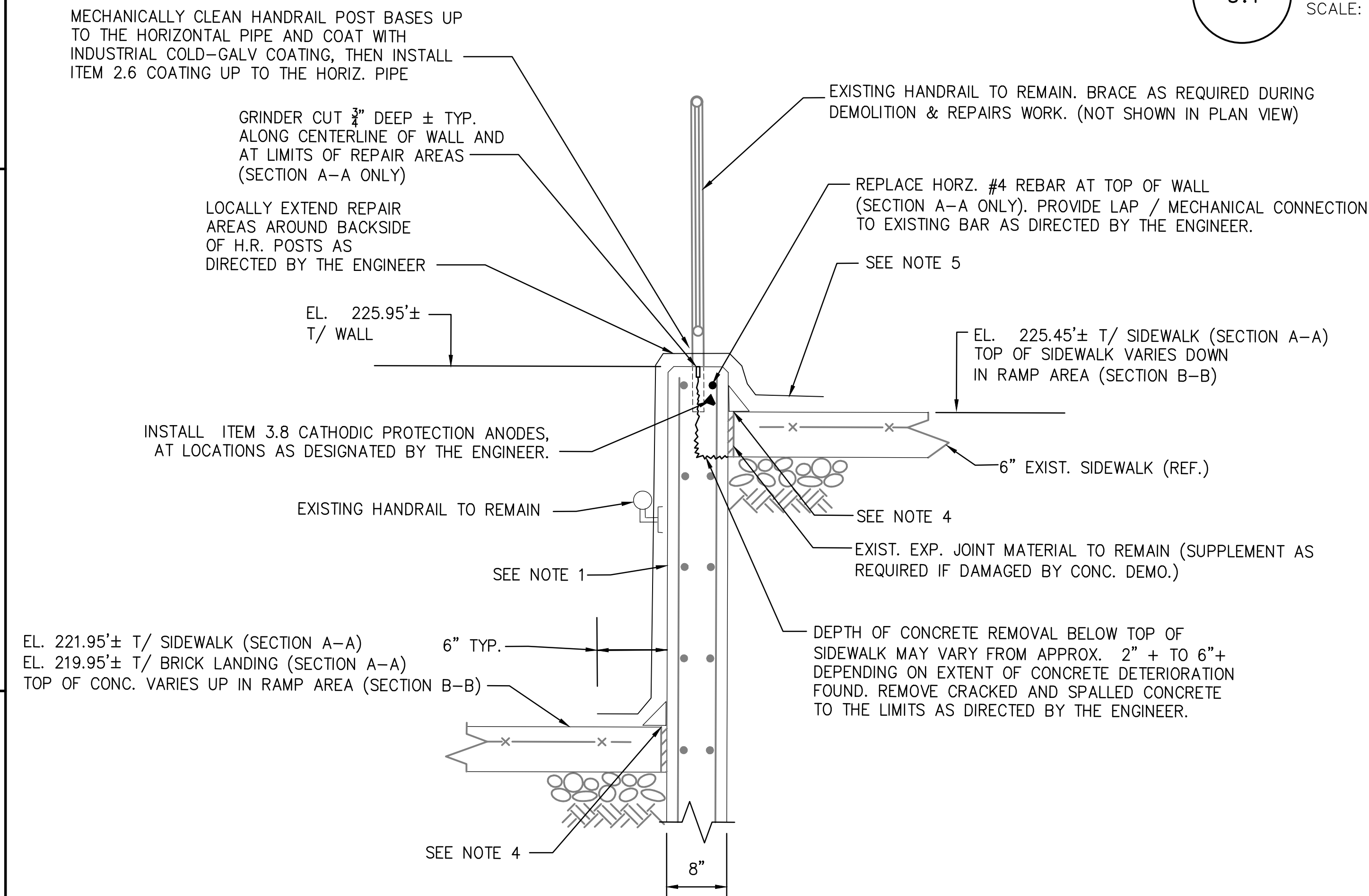


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 LAST UPDATE: Friday, April 15, 2022 5:32:55 AM
 PLOT DATE: Friday, April 15, 2022 5:34:37 AM
 ARCH D - 14-Apr-22



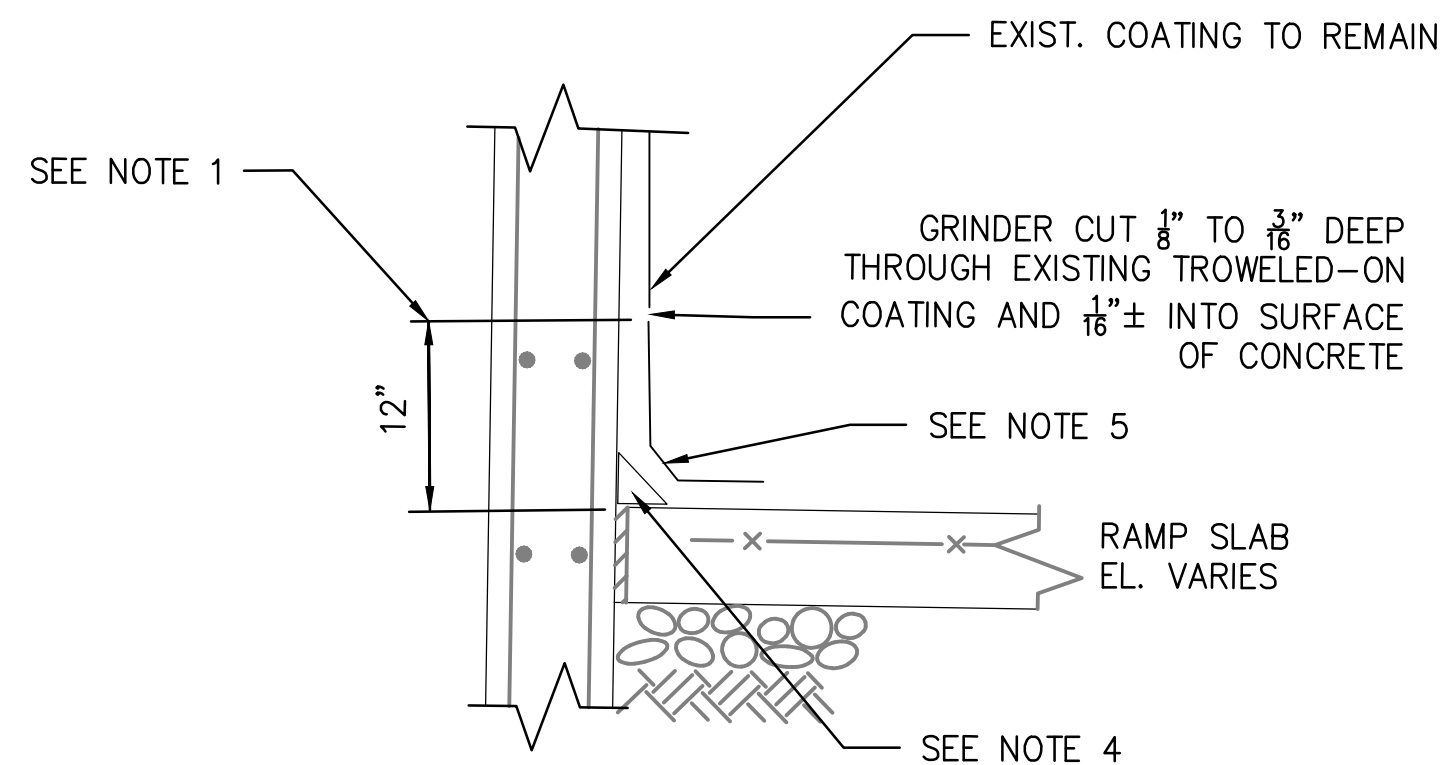
PEDESTRIAN BRIDGE PIER-B – GRADE STAIRS & RAMP REPAIRS:
 PLAN

SCALE: 1/4"=1'-0"



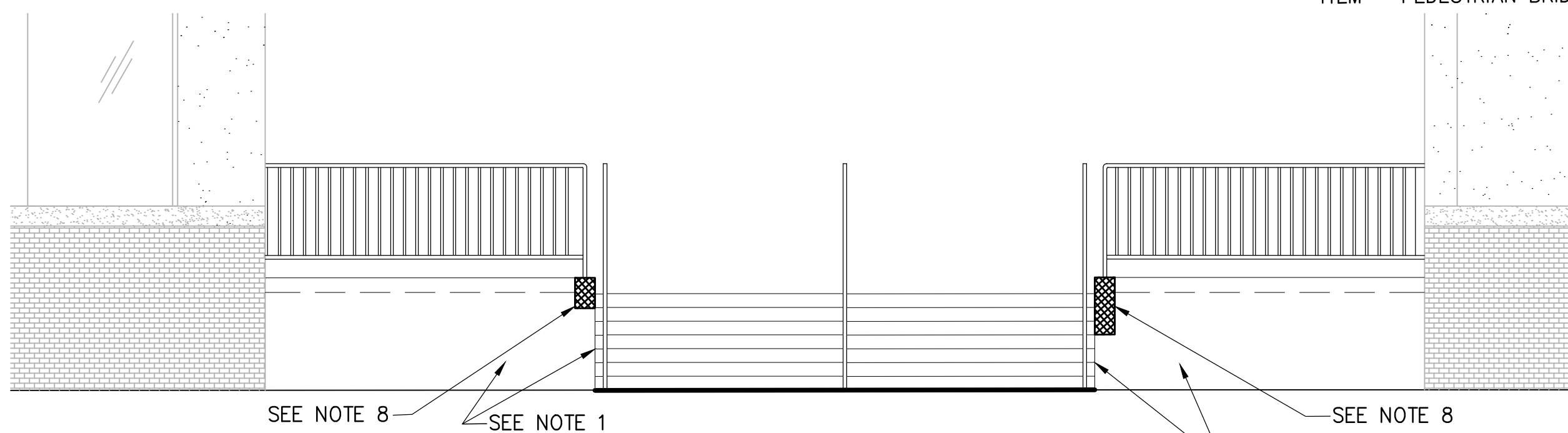
PEDESTRIAN BRIDGE PIER-B – GRADE STAIRS & RAMP REPAIRS:
 SECTION A – A (AS NOTED) ; SECTION B – B SIMILAR

SCALE: 1/4"=1'-0"



SECTION C-C

NOT TO SCALE



PEDESTRIAN BRIDGE PIER-B – GRADE STAIRS & RAMP REPAIRS:
 WEST ELEVATION (LOOKING EAST)

SCALE: 1/4"=1'-0"

NOTES:

1. REMOVE EXISTING TROWELED-ON CONTENTIOUS PROTECTIVE COATING FROM ALL EXPOSED SURFACES OF THE WALLS AND SIDES OF STAIRS (SUBSIDIARY TO ITEM 9.1) TO PROVIDE A CLEAN AND ROUGHENED CONCRETE SURFACE FOR APPLICATION OF TRAFFIC COATING UNDER ITEM 2.6.
2. FORM AND POUR NEW WALL CAP (SUBSIDIARY TO ITEM 9.1) UTILIZING APPROVED REPAIR MORTAR AS DESIGNATED FOR ITEM 2.1.
3. ABRASIVE BLAST (SUBSIDIARY TO ITEM 9.1) ALL EXISTING REBAR TO WHITE METAL AND ALL CONCRETE SURFACES TO RECEIVE TRAFFIC TOPPING COATING. APPLY EPOXY PROTECTIVE COATING TO REBAR (SUBSIDIARY TO ITEM 9.1) EXCEPT FOR AREAS WHERE CATHODIC PROTECTION ANODE CONNECTIONS WILL BE MADE.
4. REMOVE 1/2" DEPTH OF EXISTING EXP.JT. MATERIAL AND INSTALL COVE SEALANT ITEM 5.6 TYP. AT TOP AND BOTTOM OF WALLS WITHIN TRAFFIC COATING LIMITS.
5. INSTALL ITEM 2.6 – TRAFFIC COATING – PEDESTRIAN GRADE TO THE WALL AND SIDEWALK SURFACES TO LIMITS AS DESIGNATED BY THE ENGINEER.
6. INSTALL WORK AREA ISOLATION BARRICADES AND SHIELDING (SUBSIDIARY TO ITEM 9.1) AS REQUIRED TO PROTECT ADJACENT PEDESTRIAN TRAFFIC DURING CONSTRUCTION OPERATIONS. SHIELDING SHALL BE CONSTRUCTED TO WITHSTAND WIND FORCES AND OTHER LOADS THAT MAY BE PRESENT DURING CONSTRUCTION. WHEN THE RAMP SIDE IS CLOSED / CAPTURED FOR CONSTRUCTION THE OWNER WILL PROVIDE SIGNAGE TO REDIRECT RAMP-USE PEDESTRIANS ACCORDINGLY.
7. PERFORM RETAINING WALL CONCRETE REPAIRS UNDER ITEM 9.1 FIELD ITEM – PEDESTRIAN BRIDGE PIER-B – GRADE STAIRS & RAMP REPAIRS. WORK AND MATERIALS TO CONFORM TO TECHNICAL SPECIFICATIONS AS DESCRIBED IN SECTION 02000
8. DEMOLISH AND CAST PARTIAL-DEPTH WALL CORNER REPAIRS (APPROX. 3" TO 4"+ DEEP)EXTENDING APPROX. 8"+ BACK ON EACH SIDE OF THE CORNER AND APPROX. 12"+ TO 24"+ DOWN FROM THE TOP OF WALL AS MARKED AND DIRECTED BY THE ENGINEER. REPAIR SHALL BE CAST MONOLITHIC WITH THE TOP OF WALL REPAIR.
9. PERFORM LOCALIZED CONCRETE REPAIRS TO BASE OF RAMP WALL SURFACES AND OTHER WALL AREAS UNDER ITEM 3.4 OVERHEAD CONCRETE MORTAR REPAIR, ITEM 3.8 CATHODIC PROTECTION ANODES, AND ITEM 3.3 EPOXY CRACK INJECTION, AS DIRECTED BY THE ENGINEER
10. SURFACE GRIND CONC. SLAB AT TOP OF RAMP 18" TO 24" BACK FROM JOINTLINE (SUBSIDIARY TO ITEM 9.1) TO REMOVE APPROXIMATELY 1/2" TO 3/4" DEPTH OF CONC. TAPERING TO 0" TO CREATE A SMOOTH TRANSITION AT UPPER SIDEWALK SLAB. REMOVE 1/2" DEPTH OF EXPANSION JT. MATERIAL AND FILL WITH SEALANT UNDER ITEM 5.3.
11. THE FOLLOWING CONTRACT ITEMS ARE ANTICIPATED TO BE UTILIZED AT LOCATIONS AND LIMITS AS DIRECTED BY THE ENGINEER FOR REPAIRS IN THIS AREA AND WILL BE MEASURED AND PAID FOR ON A UNIT COST BASIS AS INSTALLED IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS OR AS OTHERWISE DIRECTED BY THE ENGINEER:
 - 2.6 FLOOR REPAIR – TRAFFIC TOPPING – PEDESTRIAN GRADE
 - 3.3 CEILING REPAIR – EPOXY CRACK INJECTION
 - 3.4 CEILING REPAIR – OVERHEAD CONCRETE MORTAR REPAIR
 - 3.8 CEILING REPAIR – GALVANIC CORROSION PROTECTION
 - 5.3 FLOOR SLAB JOINT REPAIR – REMOVE AND REPLACE FAILED T/T JOINT SEALANTS
 - 5.6 FLOOR REPAIR – INSTALL OR REPLACE COVE SEALANT

ALL OTHER WORK SHALL BE SUBSIDIARY TO ITEM 9.1 FIELD ITEM – PEDESTRIAN BRIDGE PIER-B – GRADE STAIRS & RAMP

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CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
 MANCHESTER - BOSTON REGIONAL AIRPORT
 PARKING GARAGE: LEVEL-4 FLOOR & LEVEL-3 CEILING
 SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
 FY22-805-50

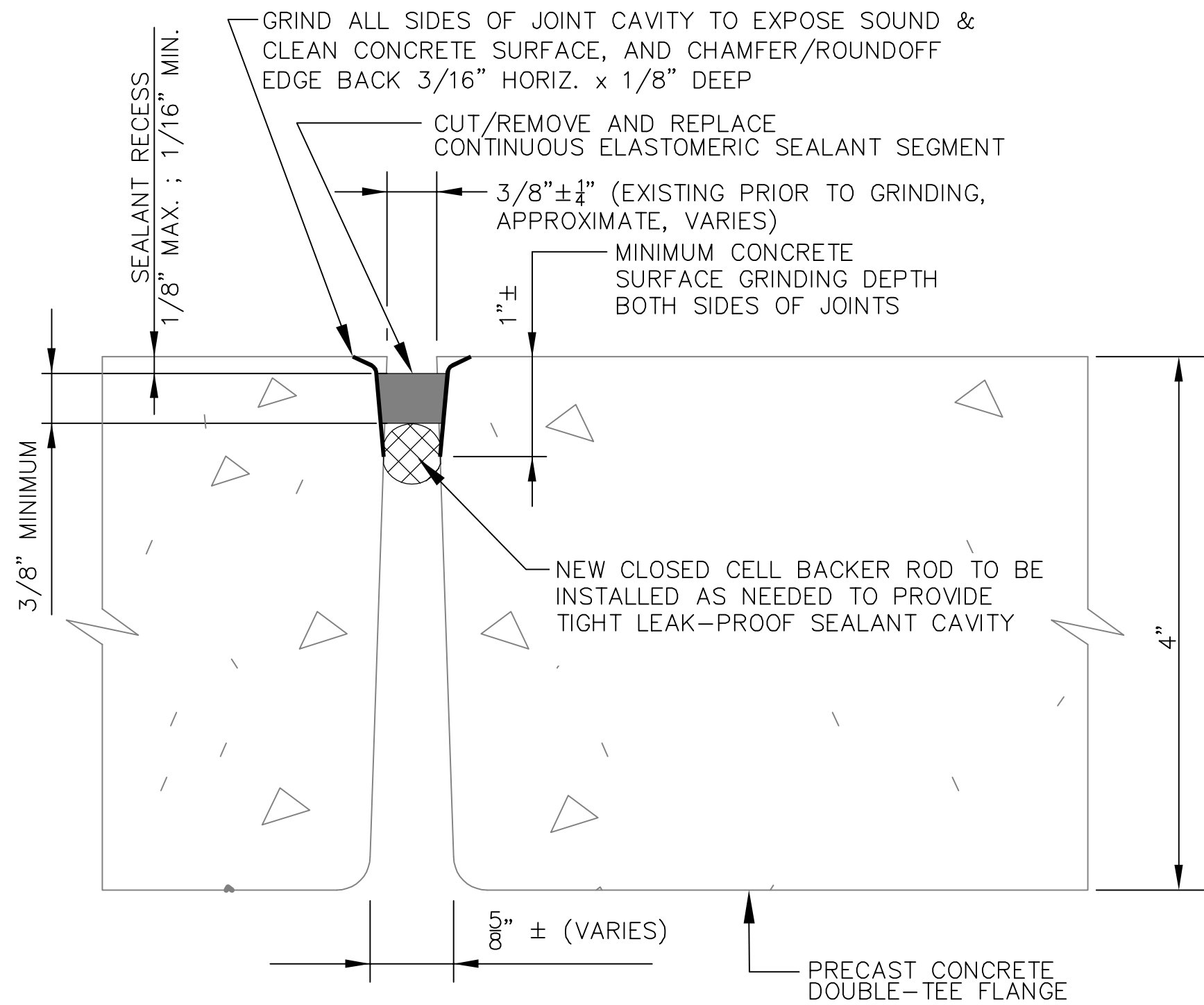
PROJECT NO: 60638480-T18
 CAD DWG FILE: 8_RP.22-PBST
 DESIGNED BY: JGG
 DRAWN BY: CMP
 DEPT CHECK: MAB
 PROJ CHECK: RJD
 DATE: APRIL 2022
 SCALE: NOT TO SCALE

RP.22-PBST.1

SHEET 8 OF 14

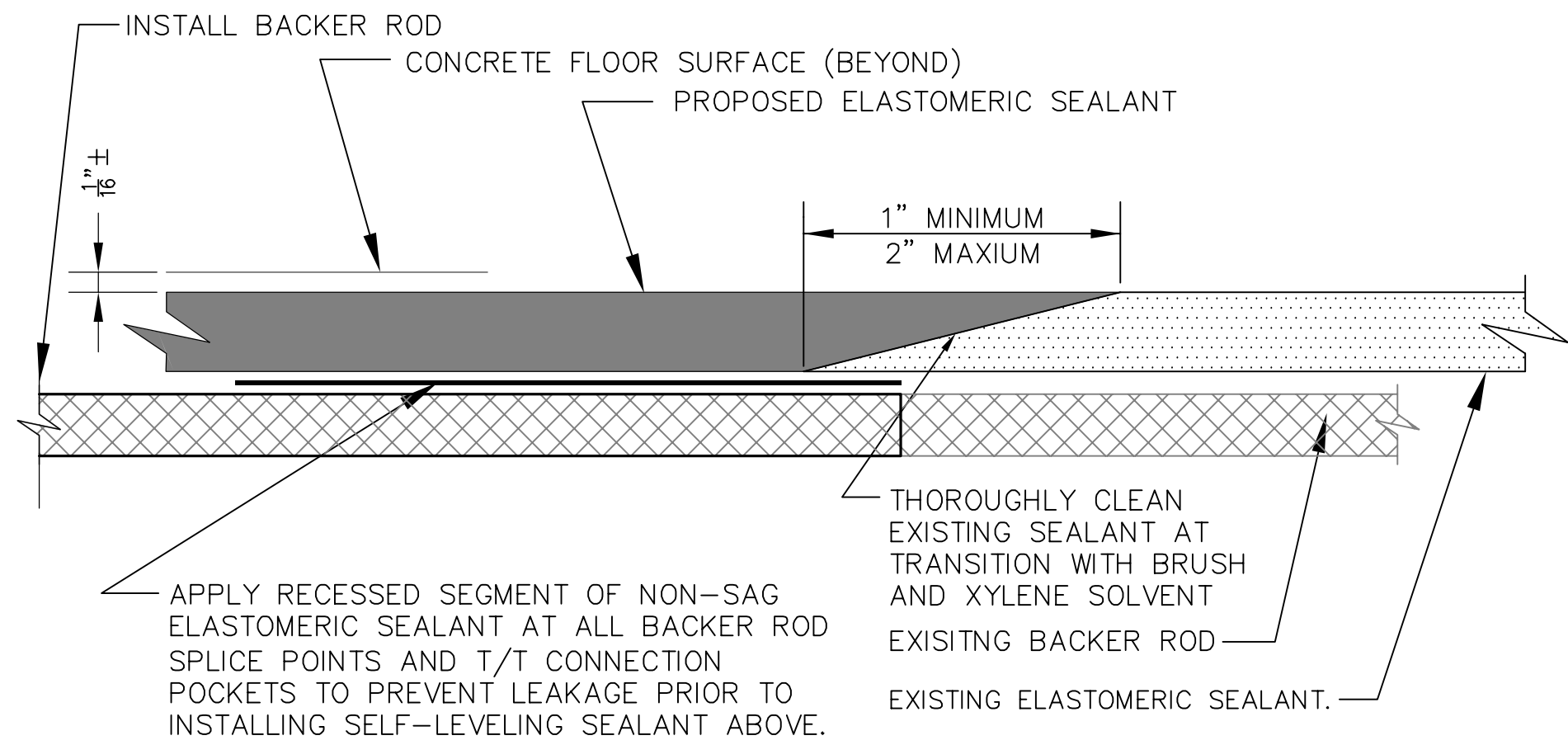
TEE-TEE FLANGE JOINT PREPARATION & SEALANT NOTES:

1. REMOVE EXISTING FAILED JOINT SEALANTS AND DEBONDED/CRACKED LOOSE CONCRETE TO SOUND CONCRETE WITHIN REPAIR AREAS.
2. IF THE DAMAGED CONCRETE REPAIR AREA EXTENDS MORE THAN $\frac{3}{4}$ " FROM THE FINISHED EDGE OF THE JOINT NOTIFY ENGINEER PRIOR TO PERFORMING REPAIRS.
3. GRIND CONCRETE SURFACES TO RECEIVE SEALANT APPLICATION. CLEAN JOINT THOROUGHLY INCLUDING BRUSH AND VACUUMING. SEALANT ADHESION SURFACES WHICH REMAIN OPEN AND ARE EXPOSED TO RAINWATER OR OTHER CONTAMINANTS SHALL BE DRIED THOROUGHLY AND WIPED CLEAN WITH SOLVENT (XYLENE OR APPROVED SEALANT MANUFACTURER RECOMMENDED ALTERNATE).
4. INSTALL CLOSED CELL BACKER ROD (AND NON-SAG ELASTOMERIC SEALANT AT BACKER ROD JOINTS) AS NEEDED TO ENSURE TIGHT LEAK-PROOF FIT. THE CONTRACTOR SHALL CLEAN ALL SEALANT LEAKS FROM THE UNDERSIDE CEILING SURFACES AND FLOOR BELOW.
5. PRIME ALL SURFACES TO RECEIVE SEALANT ADHESION WITH SEALANT MFR.'S RECOMMENDED PRIMER.
6. INSTALL SELF-LEVELING ELASTOMERIC SEALANT (SIKAFLEX 2C-SL, AS MFD BY SIKA CORP OR APPROVED EQUAL) IN ACCORDANCE WITH MANUFACTURER'S WRITTEN INSTRUCTIONS. FINISHED SURFACE OF SEALANT SHALL BE $\frac{1}{8}$ " TO $\frac{3}{8}$ " (MAX.) BELOW TOP OF PRECAST CONCRETE FLOOR. DEPTH OF SEALANTS ($\frac{3}{8}$ " MIN.) SHALL BE HALF OF THE JOINT WIDTH TO BE SEALED UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
7. EACH SEGMENT OF NEW SEALANT SHALL BE PLACED CONTINUOUSLY WITHOUT ALLOWING IN-PLACE SEALANTS TO SKIN-CURE BEFORE INSTALLING CONTINUATION OF THE SEALANT SEGMENT.
8. AFTER COMPLETION OF SEALANT REPLACEMENTS THE CONTRACTOR SHALL WATER TEST JOINTLINES TO VERIFY LEAK-TIGHT FLOOR JOINTS PRIOR TO SHOT-BLAST CONCRETE SURFACE PREPARATION AND TRAFFIC TOPPING MEMBRANE COATING SYSTEM.



TYPICAL SECTION AT JOINT SEALANTS
(BEYOND CONNECTION)
SCALE: FULL

NOTE:
CONTRACTOR SHALL VERIFY AND
COORDINATE THAT THE EXTENTS OF
ALL REPAIRS ARE WITHIN CAPTURED
SAFETY AREAS ON THE LEVEL BELOW



SEALANT TRANSITION DETAIL
(PROPOSED / EXISTING)

FLOOR SLAB JOINT REPAIR –
REMOVE AND REPLACE T/T FLANGE JOINT SEALANTS

5.3
SCALE: AS NOTED

NOTES (ITEM 5.2A):

- 1.) SAWCUT & REMOVE EXISTING NOSING MATERIAL FROM DESIGNATED REPAIR AREA TO SOUND SUBSTRATE MATERIAL (TOP OF EXPANSION JOINT GLAND FLANGE & HOLD-DOWN STRIP).
- 2.) REMOVE HOLD-DOWN FASTENER HEADS & WASHERS WITHIN THE REPAIR AREA BY GRINDING. HOLD-DOWN STRIP TO REMAIN. INSTALL NEW STAINLESS STEEL FASTENERS AS REQUIRED TO HOLD THE EXPANSION JOINT GLAND FLANGE IN PLACE AND KEEP THE ALUMINUM STRIP FIRMLY AT THE BOTTOM OF THE NOSING MATERIAL.
- 3.) PREPARE EXPOSED SURFACES FOR ELASTOMERIC CONCRETE NOSING MATERIAL BY SANDBLASTING OR MECHANICAL MEANS ACCORDING TO MANUFACTURERS RECOMMENDATIONS.
- 4.) INSTALL ELASTOMERIC CONCRETE NOSING MATERIAL IN ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS.

WORK ITEM 5.2A

NOSING SURFACE REPAIR: (LENGTH VARIES FROM 1'-0" TO 4'-0"±) SAWCUT MAXIMUM OF $\frac{3}{8}$ " DEEP AT LONGITUDINAL AND TRANSVERSE LIMITS OF NOSING MATERIAL TO BE REMOVED. USE CAUTION WHEN SAWCUTTING TO AVOID DAMAGE TO EXPANSION JOINT GLAND FLANGES.

EXISTING 1" WIDE ALUMINUM HOLD-DOWN STRIP WITH STEEL FASTENER & 1" DIA. STEEL WASHERS @ 12" C/C. REMOVE (GRIND-OFF) FASTENER HEAD AND WASHER.

INSTALL ELASTOMERIC CONCRETE NOSING MATERIAL

1" EXISTING NOSING MATERIAL BLOCKOUT

1 1/2" CONCRETE WASH

4"

7 1/2" EXISTING CONCRETE WASH

3" EXISTING NOSING BLOCKOUT

LIMITS OF WORK ITEM 2.7A TO BE APPLIED AT BOTH SIDES OF EXPANSION JOINTS AS DIRECTED BY THE ENGINEER. TERMINATE COATING EDGE IN $\frac{1}{4}$ " DEEP SAWCUT.

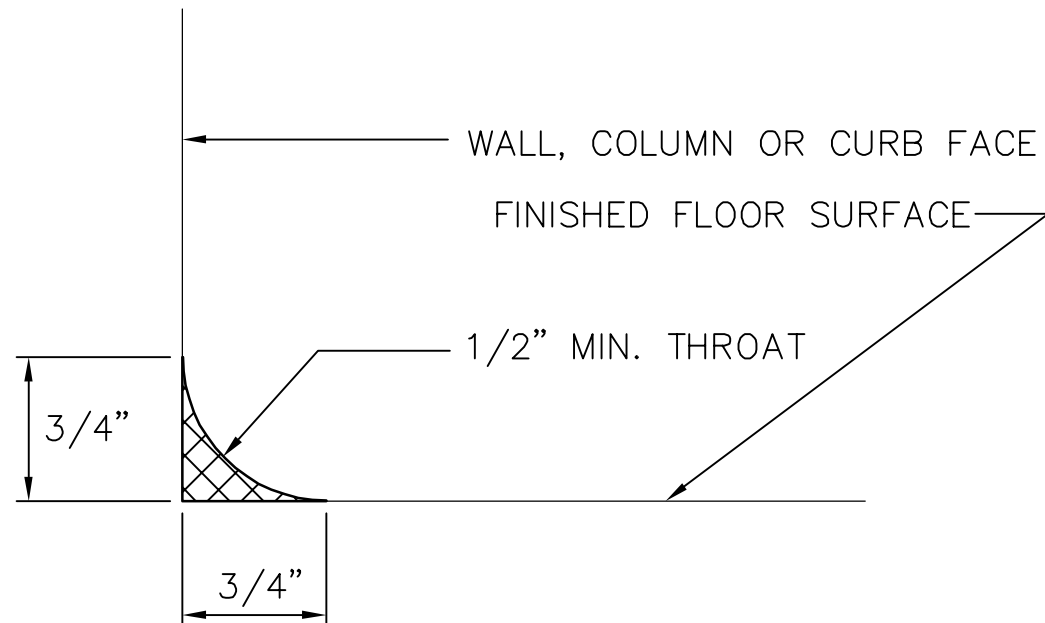
PRECAST CONCRETE DOUBLE TEE FLANGE (TYP.)

EXISTING EXPANSION JOINT GLAND (TO REMAIN). TREMCO DEKTITE EXP. JOINT SYSTEM; MODEL #525 NTS. 2" EXP. JOINT AT LINES 8 & 20

FLOOR SLAB JOINT REPAIR –
EXPANSION JOINT NOSING SURFACE REPAIR

5.2A

SCALE: NTS



NOTES (ITEM 5.6):

1. PREPARE CONCRETE SURFACES BY GRINDING AND OTHER SURFACES BY SOLVENT WIPE (XYLENE OR APPROVED EQUAL.). APPLY PRIMER, AND ALLOW PRIMER TO CURE PER MANUF. RECOMMENDATIONS, PRIOR TO APPLYING SPECIFIED SEALANTS.
2. SEE SPECIFICATIONS FOR APPROVED MATERIALS.

FLOOR SLAB JOINT REPAIR –
INSTALL OR REPLACE COVE SEALANT

5.6

SCALE: NTS

LEGEND



WORK ITEM NUMBER

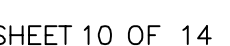
CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE: LEVEL 4 FLOOR & LEVEL-3 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY22-805-50

FLOOR REPAIR DETAILS - I

PROJECT NO: 60638480-T18
CAD DWG FILE: 9-11_RD.22-1-3
DESIGNED BY: JGG
DRAWN BY: CMP
DEPT CHECK: MAB
PROJ CHECK: RJJ
DATE: APRIL 2022
SCALE: AS NOTED

RD.22-F1

SHEET 9 OF 14



ARCH. D. - 14-Apr-22

NOTES (ITEM 2.7B):

1.) IN AREAS WHERE ITEM 2.7J FLOOR REPAIR - TRAFFIC TOPPING AT PRECAST TEE-TO-TEE FLANGE EDGE JOINTS, NYLON REINFORCED IS APPLIED ON TOP OF EXISTING COATINGS AND PRIOR TO ITEM 2.7B, THE SURFACE OF THE EXISTING COATING SHALL BE PREPARED BY DETERGENT CLEANING STAINED AREAS AND POWER WASHING ALL SURFACES, FOLLOWED BY MECHANICAL ABRASIVE MEANS (BRUSH BLAST WITH ABRASIVE MEDIA) AND WIPED WITH XYLENE (OR APPROVED EQUAL) SOLVENT, IN ACCORDANCE WITH THE COATING MANUFACTURER'S RECOMMENDATIONS.

2.) THE PAY QUANTITY AREA FOR ITEM 2.7B WILL BE REDUCED FOR THE EQUIVALENT AREA OF ITEM 2.7J APPLIED WITHIN THE ITEM 2.7B LIMITS AS DIRECTED.

FLOOR REPAIR - TRAFFIC TOPPING - WEARCOAT AND UV-TOPCOAT

SCALE: 3/4" = 1'-0"

LEGEND



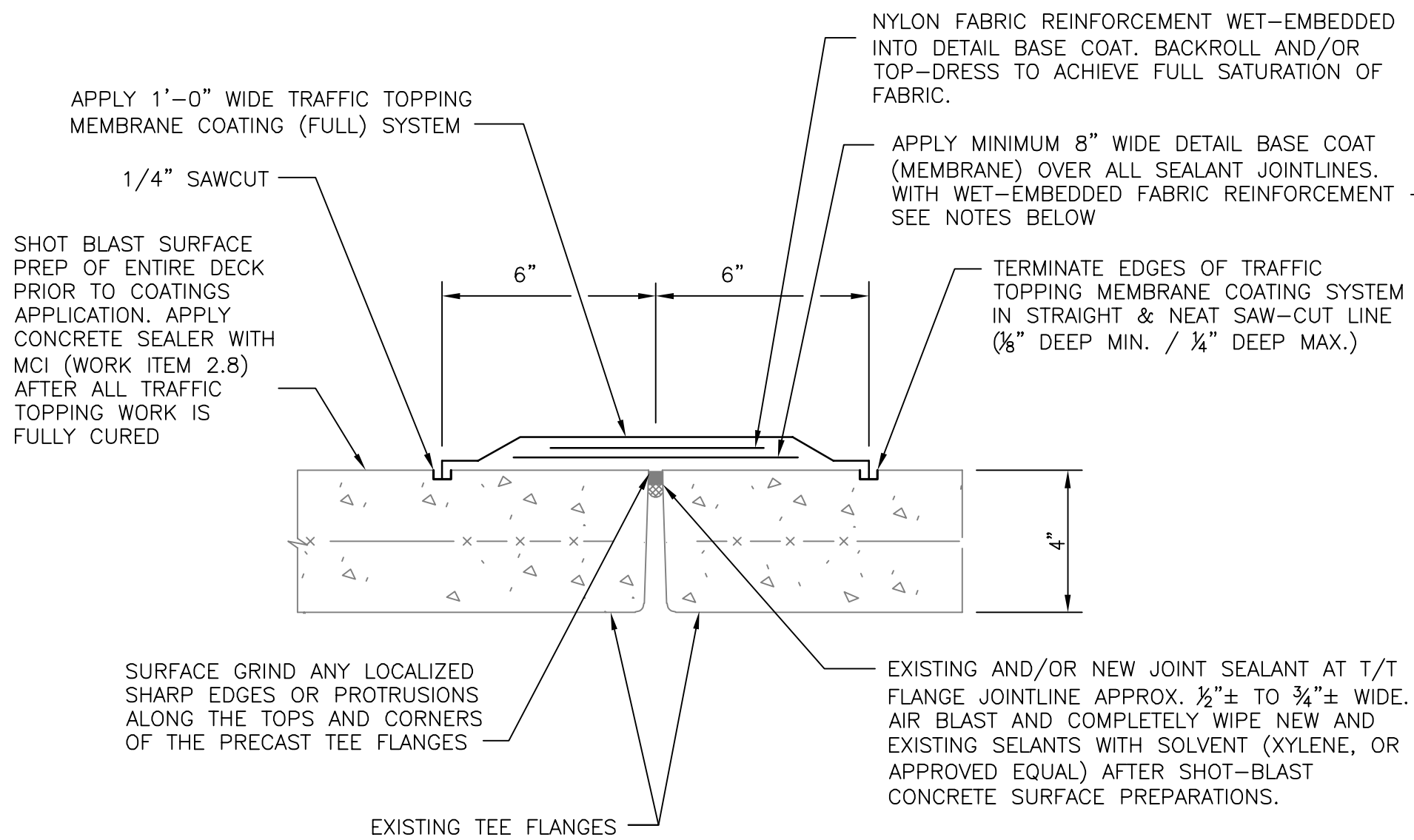
WORK ITEM NUMBER

NOTES (ITEM 2.7J):

- RECESSES, DEPRESSIONS, OR IRREGULARITIES ALONG THE JOINTLINES SHALL BE FILLED TO PROVIDE A SMOOTH LEVEL COATING TRANSITION BETWEEN PRECAST TEE FLANGES ACROSS THE JOINTLINES. PROVIDE A 100% SOLIDS FLEXIBLE EPOXY FILLER PASTE MATERIAL AS NEEDED PRIOR TO THE DETAIL BASE COAT -OR- THE DETAIL BASE COAT APPLICATION SHALL BE APPLIED IN THICKNESS AS REQUIRED ACROSS THE JOINTLINES TO FILL THE JOINTLINE AND PROVIDE THE SMOOTH TRANSITION (IN ACCORDANCE WITH THE COATING SYSTEM MANUFACTURER RECOMMENDATIONS).
- A 6" NYLON FABRIC REINFORCEMENT STRIP SHALL BE WET-EMBEDDED AND BACKROLLED INTO THE DETAIL BASE COAT TO BE SUPPLIED BY THE COATING SYSTEM MANUFACTURER AS PART OF THE COATING SYSTEM TO PROVIDE JOINT-BRIDGING STRENGTH FOR A 5-YEAR WARRANTY OF THE COATING SYSTEM FOR THIS APPLICATION.
- SEE SPECIFICATIONS FOR APPROVED MATERIALS.
- ALL COATING EDGES SHALL BE MASKED/TAPED OR OTHERWISE GUARDED TO ENSURE NEAT, STRAIGHT, AND TRUE LINES. MASKING SHALL BE REMOVED PRIOR TO COATING COMPONENT CURE AS REQUIRED TO PREVENT DAMAGE OR MASKING TAPE RESIDUE. EXCESS COATING OR TAPE RESIDUE SHALL BE REMOVED BY GRINDING AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST TO THE OWNER.
- WHERE EXISTING ITEM 2.7J MUST BE REMOVED MAKE EDGE CUTS (SAW OR GRINDER) AND REMOVE FULL COATING SYSTEM DOWN TO CONCRETE BY MECHANICAL MEANS (GRINDER WHEEL, ABRASIVE BLAST, SCABBLER, SHOT BLAST, OR HYDRO-REMOVAL) TO CLEAN CONCRETE SURFACE OR SOUND SUBSTRATE AS APPROVED BY THE ENGINEER SUBSIDIARY TO THE WORK UNDER ITEM 2.7J AND/OR ITEM 1.1.

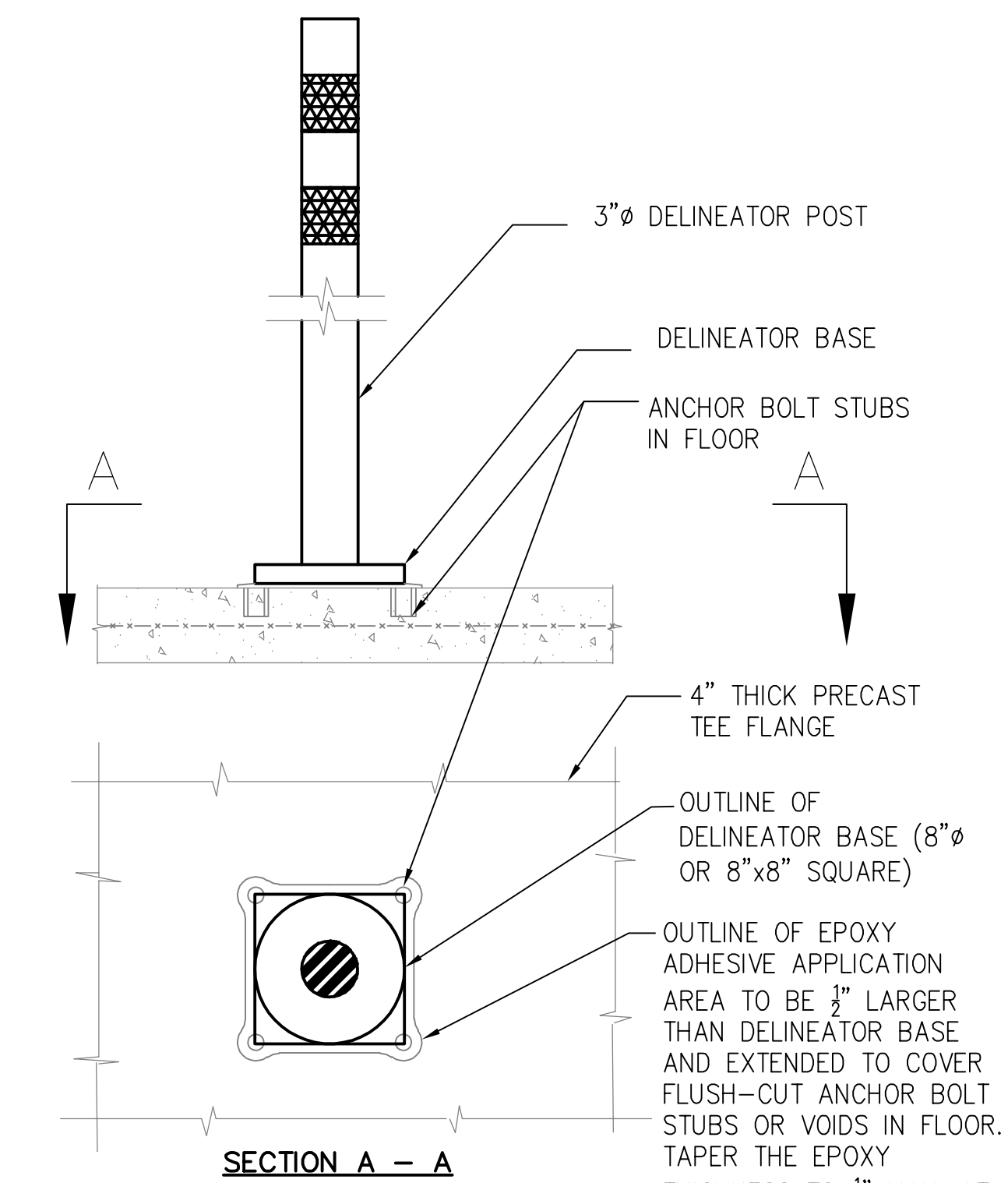
FLOOR REPAIR - TRAFFIC TOPPING AT JOINTLINE PRECAST TEE-TO-TEE FLANGE EDGE JOINT, NYLON REINFORCED

SCALE: 3"=1'-0"



AT LOCATIONS AS INDICATED ON THE REPAIR PLANS AND/OR AS DIRECTED BY THE ENGINEER, (WHERE TRAFFIC COATING ALREADY EXISTS) PROVIDE WORK ITEM 2.7B - TRAFFIC TOPPING WEAR COAT & UV TOPCOAT OVER THE 8-FOOT WIDE CENTER STRIP THAT HAS AN EXISTING COATING.

PRIOR TO TRAFFIC TOPPING WORK ITEM 2.7B INSTALLATION, PERFORM WORK REQUIRED FOR ALL CONCRETE FLOOR AND CEILING REPAIRS AND ITEM 2.7J (1'-0" WIDE) IN LOCALIZED AREAS AS INDICATED ON THE PLANS AND/OR AS DIRECTED BY THE ENGINEER. SEE NOTE 1.

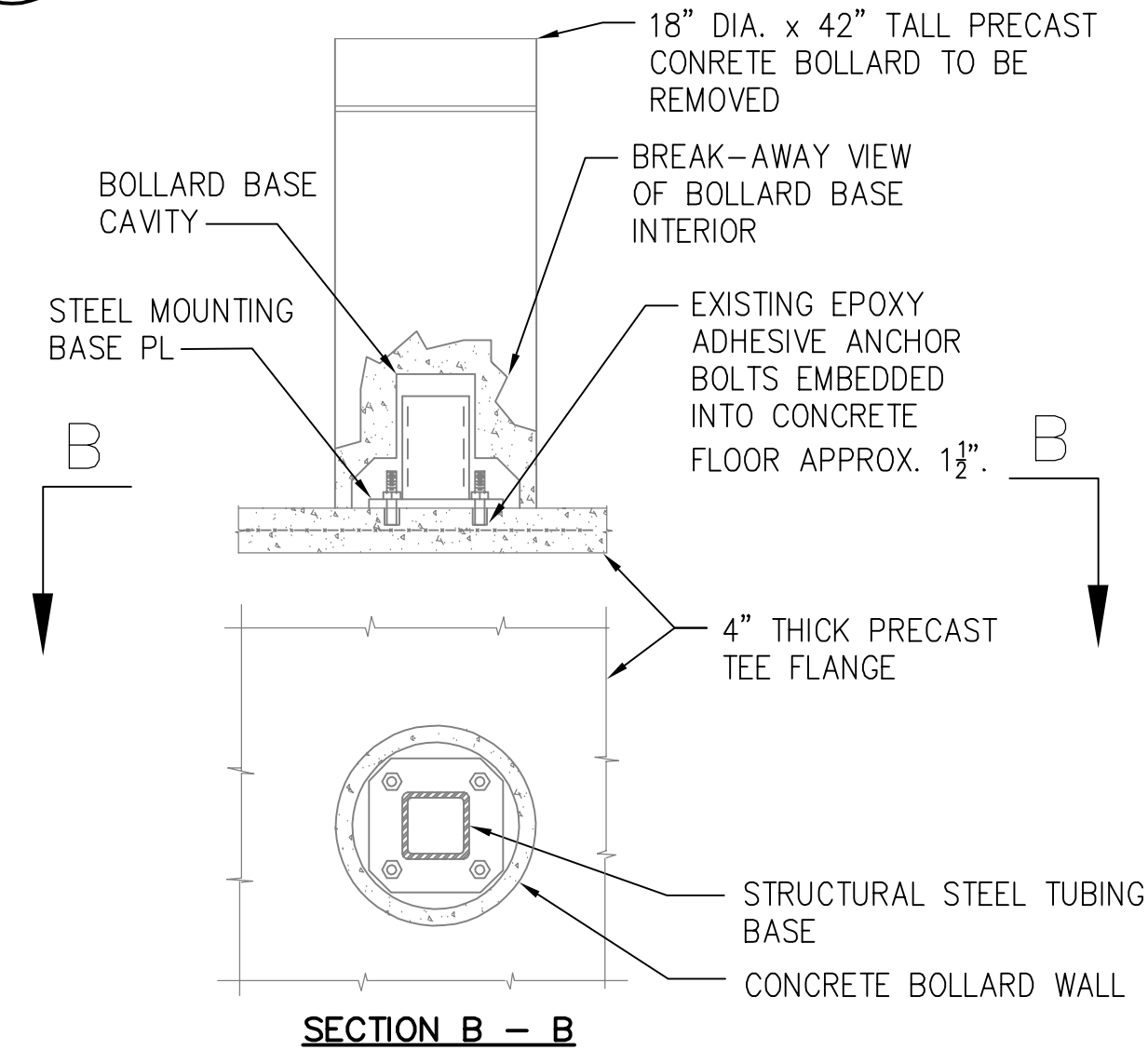


NOTES (ITEM 8.4):

- PREPARE SURFACE AND INSTALL TRAFFIC DELINEATORS IN ACCORDANCE WITH THE EPOXY ADHESIVE MFR. INSTRUCTIONS.
- TRAFFIC DELINEATOR INSTALLATIONS AT LOCATIONS WHERE NO TRAFFIC COATINGS ARE TO BE APPLIED (AT BARE CONCRETE) SHALL INCLUDE COVERING THE FLUSH-CUT ANCHOR BOLT STUBS WITH EPOXY ADHESIVE SUBSIDIARY TO THE WORK.

BOLLARDS & TRAFFIC CONTROL - INSTALL TRAFFIC DELINEATOR DEVICES

SCALE: NTS



NOTES (ITEM 8.1):

- LIFT AND REMOVE CONCRETE BOLLARD FROM STEEL BASE POST, REMOVE ANCHOR BOLT NUTS AND STEEL BASE. USE CAUTION TO AVOID DAMAGE TO FLOOR.
- CUT-OFF EXISTING ANCHOR BOLTS FLUSH TO FLOOR AND GRIND SMOOTH WITH SLIGHT RECESS INTO (BELOW) FLOOR TEXTURE RIDGES. REMOVE ALL BURRS AND SHARP EDGES.
- ANCHOR BOLT HOLES FOR ANCHORS THAT PULL OUT OF THE FLOOR (ADHESIVE BONDING FAILURE) SHALL BE THOROUGHLY MECHANICALLY CLEANED AND FILLED WITH NON-SHRINK GROUT.
- COAT OVER THE CUT-OFF BOLTS AND FILLED HOLES WHERE TRAFFIC TOPPINGS ARE DESIGNATED FOR INSTALLATION.
- REPAIR ANY PREVIOUSLY DAMAGED (SPALLED) FLOOR AREAS UNDER WORK ITEM 2.3. ANY AREAS THAT ARE DAMAGED BY BOLLARD REMOVAL WORK SHALL BE REPAIRED IN ACCORDANCE WITH ITEM 2.3 AT NO EXPENSE TO THE OWNER.
- ALL BOLLARDS SHALL BE REMOVED FROM THE GARAGE AND NEATLY PLACED AT THE MAINTENANCE DRIVEWAY AREA LOCATED AT THE NORTH END OF THE PARKING GARAGE AS DIRECTED BY THE OWNER. CONTRACTOR SHALL DISPOSE OF STEEL BASES SUBSIDIARY TO THIS WORK. DISPOSAL OF CONCRETE BOLLARDS IS UNDER WORK ITEM 8.2.

BOLLARDS & TRAFFIC CONTROL - CONCRETE BOLLARD REMOVAL

SCALE: NTS

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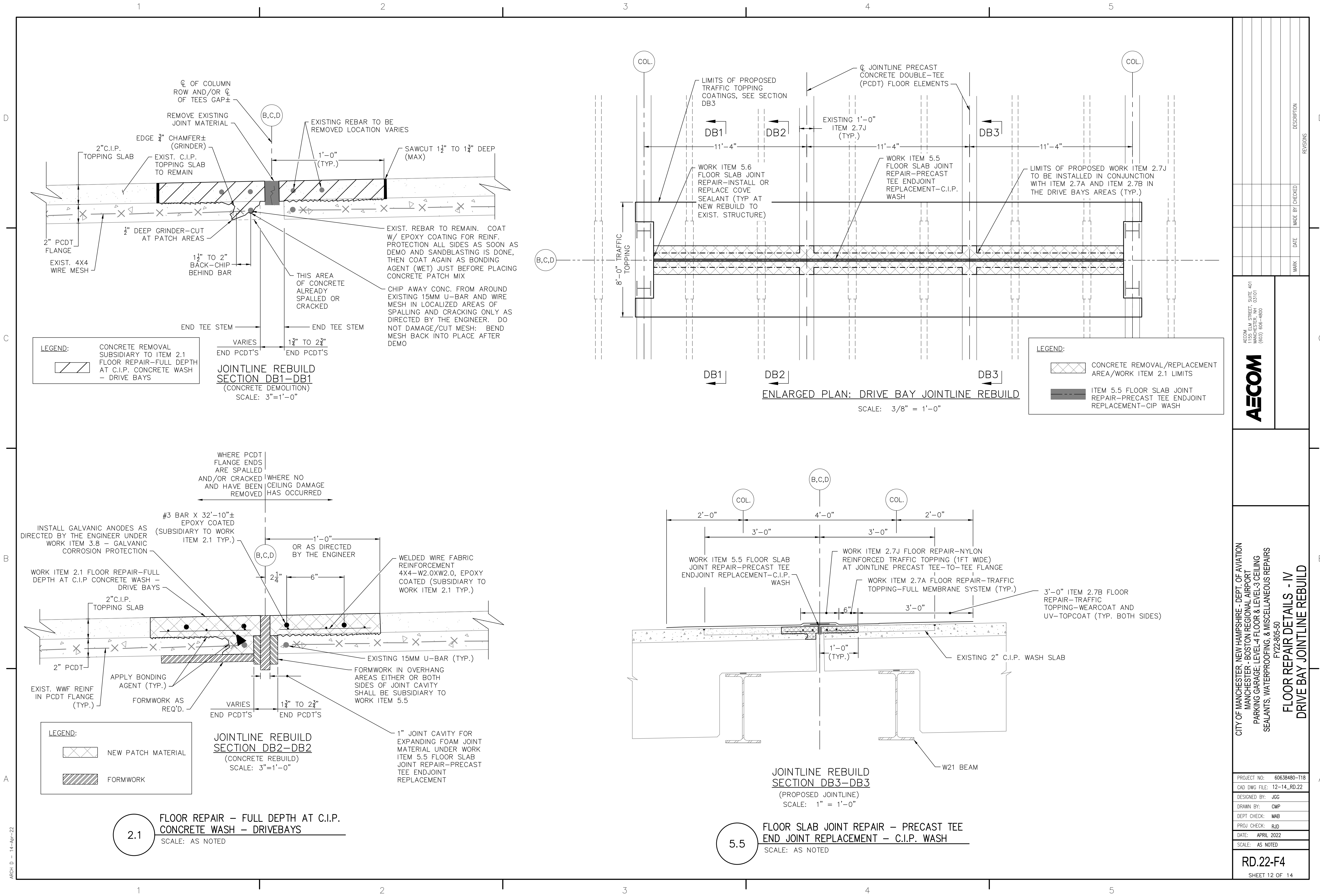
CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL 4 FLOOR & LEVEL-3 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY22-805-50

FLOOR REPAIR DETAILS - III

PROJECT NO: 60638480-T18
CAD DWG FILE: 9-11_RD.22-1-3
DESIGNED BY: JGG
DRAWN BY: CMP
DEPT CHECK: MAB
PROJ CHECK: RJD
DATE: APRIL 2022
SCALE: AS NOTED

RD.22-F3

SHEET 11 OF 14



2.1 FLOOR REPAIR - FULL DEPTH AT C.I.P. CONCRETE WASH - DRIVEBAYS
SCALE: AS NOTED

5.5 FLOOR SLAB JOINT REPAIR - PRECAST TEE END JOINT REPLACEMENT - C.I.P. WASH
SCALE: AS NOTED

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CITY OF MANCHESTER, NEW HAMPSHIRE - DEPT. OF AVIATION
MANCHESTER - BOSTON REGIONAL AIRPORT
PARKING GARAGE - LEVEL-4 FLOOR & LEVEL-3 CEILING
SEALANTS, WATERPROOFING, & MISCELLANEOUS REPAIRS
FY22-805-50

FLOOR REPAIR DETAILS - IV
DRIVE BAY JOINTLINE REBUILD

PROJECT NO: 60638480-T18
CAD DWG FILE: 12-14_RD.22
DESIGNED BY: JGG
DRAWN BY: CMP
DEPT CHECK: MAB
PROJ CHECK: RJD
DATE: APRIL 2022
SCALE: AS NOTED

RD.22-F4
SHEET 12 OF 14

NEW PATCH MATERIAL

