

ADDENDUM No. 1
BID DOCUMENTS FOR
MANCHESTER • BOSTON REGIONAL AIRPORT
PRE-CONDITIONED AIR UNITS & GROUND POWER EQUIPMENT REPLACEMENTS
PROJECT
MHT / City Bid # FY22-805-51
AIP # 3-33-0011-TBD-2022

May 6, 2022

This document and its attachments shall become an integral part of the Project Bid Documents as originally issued on April 25, 2022. Conformed Project Documents, if applicable, may be issued to the successful bidder at the time of contract execution.

The information contained in this Addendum shall supplement and/or supersede information contained in Bid Documents issued previously for this project as described herein.

The Bidder shall acknowledge receipt of this addendum on the form attached to the Proposal (Bid) Documents submittal entitled **ACKNOWLEDGMENT OF ADDENDA**.

This Addendum includes:

BIDDER QUESTIONS AND/OR RFI'S:

There have been no Bidder Questions or RFI's received as of the date and time of the issuance of this Addendum (via email) to all Bidders - 5/6/2022 1:05 PM .

CLARIFICATIONS AND MODIFICATIONS TO THE BID DOCUMENTS:

1. Revise Volume I - Bid Proposal & Contract, Specification Section 00030 Bid Advertisement and Specification Section 00100 Instruction To Bidders Subsection 1.25 ADDENDA AND INTERPRETATIONS to indicate the following:

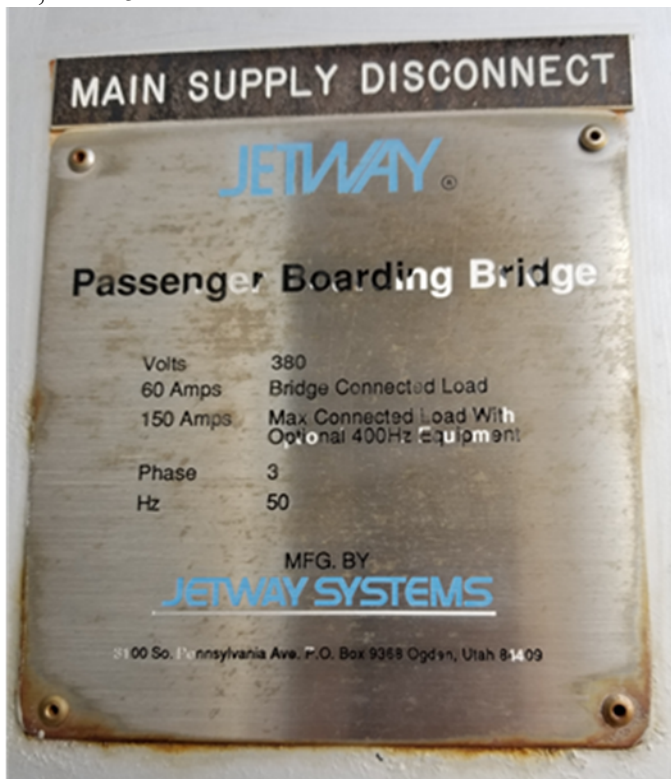
All Bid-Related Inquiries shall be submitted in writing and received on or before 3:00 pm on Thursday May 19, 2022, to John G. Goudreault, P.E., Associate Vice President at AECOM, via email to john.goudreault@aecom.com.

Any and all such interpretations and any supplemental instructions will be in the form of written addenda to the specifications which, if issued, **will be emailed to the prospective bidders (at the respective email address furnished to the Airport for such purposes), not later than 5:00 pm on Friday May 20, 2022** and will subsequently be posted to the Airport's website at www.flymanchester.com.

2. Volume III - Technical Specifications, Section 23 75 15, AIRCRAFT PRE-CONDITIONED AIR UNIT (PCA) EQUIPMENT, Subsection 2.4 COMPONENTS, E. Airport Communications: b. shall be replaced to read:

b. Communication connection to the BMS shall be hardwired. Hardwired communication shall be incorporated into the proposed Pantograph Cable Management System (PGH-CMS) as necessary. The communication cable from the PCA Unit shall be routed through the PGH-CMS to a new junction box to be mounted to the PBB Rotunda then via rigid galvanized steel (RGS) conduit to a new junction (termination) box to be mounted on the exterior of the Terminal building wall directly behind the PBB Rotunda. The interconnection of the PCA communication cable to the BMS will be performed by the Owner at a later date.

3. Volume III - Technical Specifications, APPENDIX A: At the Pre-Bid Meeting it was pointed out that on the Electrical One-Line Diagram (Figures 6-B and 6-C) showing Existing Conditions & Demolition for Gate 10, 14, and 15, the informational note for the existing Passenger Boarding Bridge Panel Main Supply Disconnect indicates 380V, 50Hz, 3Phase. This information is directly transcribed from the Manufacturer's Data Plate mounted to the face of existing panel as shown in the photo below, however, the available power supply is 480V, 60Hz, 3Phase. The differentiation (380V, 50Hz) in the informational note is essentially irrelevant to the Bid, as this existing panel will be demolished and replaced with a new 480V, 60Hz, 3Phase panel as indicated on the Electrical One-Line Diagram (Figures 7-B and 7-C) showing PROPOSED Conditions for Gate 10, 14, and 15.



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